

PALM BEACH COUNTY TYPICAL DETAILS FOR PAVEMENT MARKINGS, SIGNING & GEOMETRICS

SEND SUGGESTIONS OR COMMENTS TO:
ENG-STANDARDDETAILS@PBC.GOV



TRAFFIC DIVISION

PALM BEACH COUNTY
DEPARTMENT OF ENGINEERING
AND PUBLIC WORKS

EFFECTIVE: JULY 2024

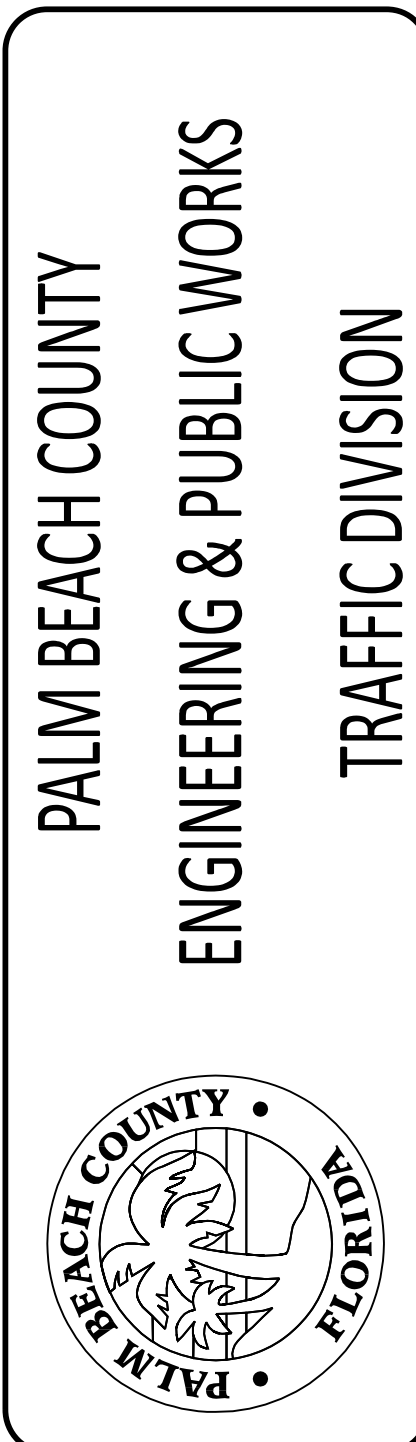
NOTE:

SEE ROADWAY PRODUCTION PALM BEACH COUNTY THOROUGHFARE ROADS TYPICAL SECTIONS FOR LANE LAYOUT AND
RIGHT-OF-WAY REQUIREMENTS ON STANDARD SECTIONS AND EXPANDED INTERSECTIONS.
SEE PALM BEACH COUNTY TRAFFIC SIGNAL INSTALLATION STANDARDS AND DETAILS FOR SIGNAL STANDARDS AND DETAILS.

INDEX OF SHEETS

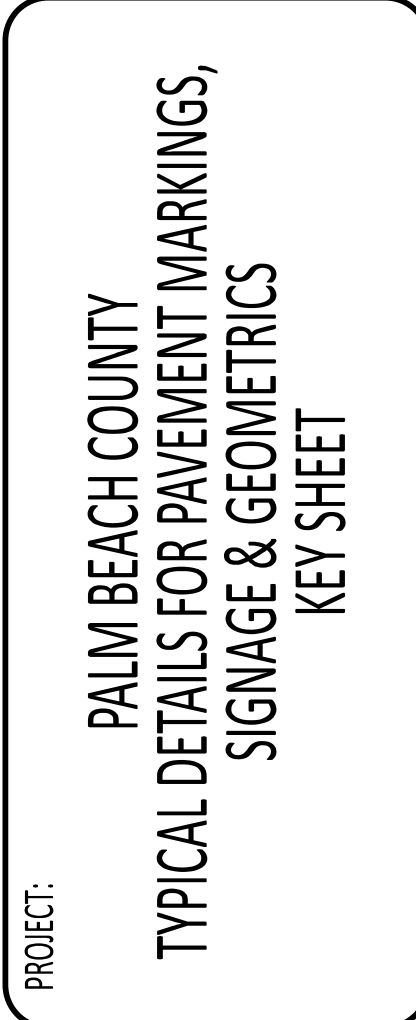
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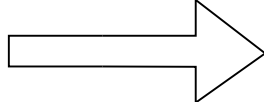
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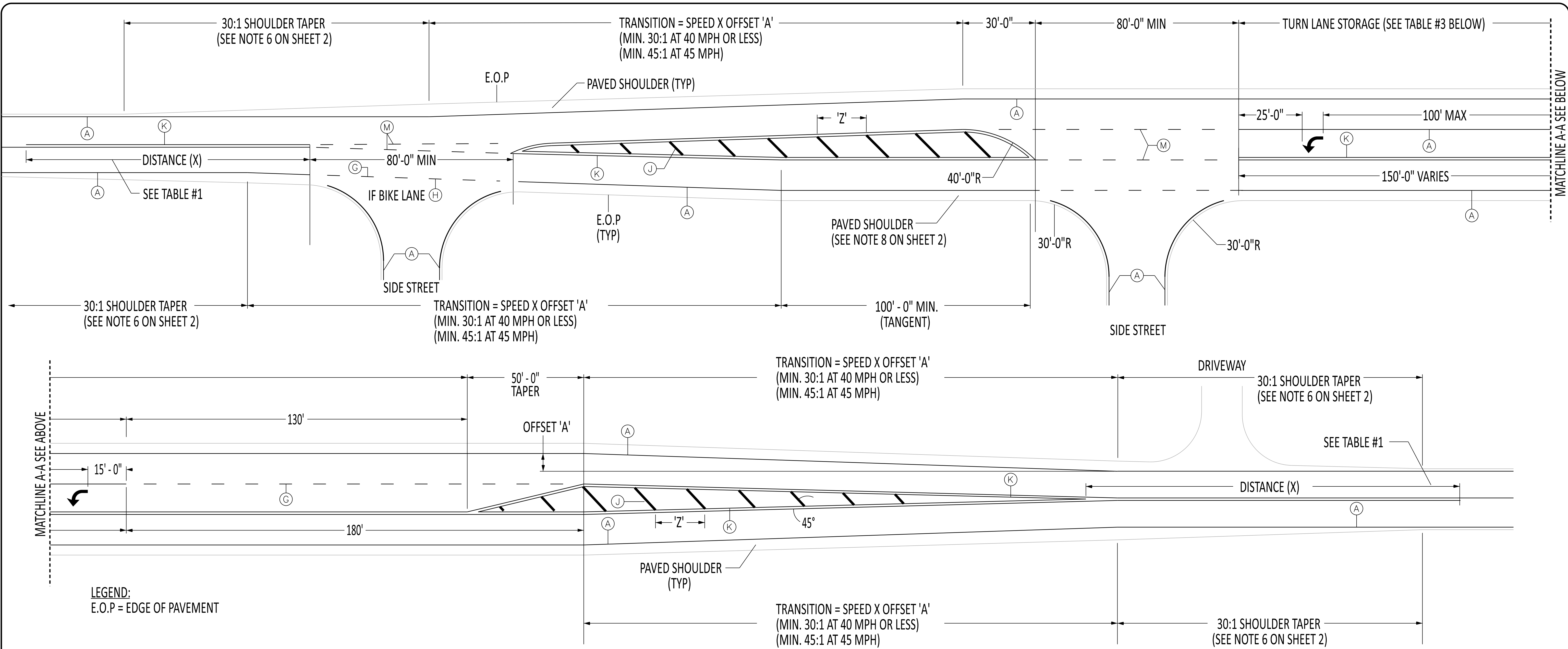


SHEET:	1
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NO.	T-P-24

GENERAL NOTES:

1. EXISTING ROAD SURFACE SHALL BE OVERLAID FOR ONE LANE WIDTH THROUGHOUT THE LIMITS OF CONSTRUCTION IF THE EXISTING PAVEMENT MARKINGS ARE INCONSISTENT WITH THOSE PROPOSED UNLESS OTHERWISE APPROVED BY THE DIRECTOR OF TRAFFIC DIVISION.
2. SPEED EQUALS POSTED SPEED IN M.P.H. OR DESIGN SPEED, WHICHEVER IS GREATER.
3. DIMENSIONS AND GEOMETRIC LAYOUTS INDICATED IN THIS DOCUMENT REPRESENT MINIMUM REQUIREMENTS AND DO NOT SUPERSEDE THE NEED FOR FURTHER ENGINEERING DESIGN TO MEET THE NEEDS OF SPECIFIC PROJECTS.
4. ALL EXISTING ABOVE GROUND UTILITIES AND ANY OBJECTS WITHIN THE RIGHT-OF-WAY OR RECOVERY AREA, WHICHEVER IS GREATER (WITHIN THE LIMITS OF CONSTRUCTION), SHALL BE SHOWN ON THE PAVEMENT MARKING PLANS.
5. ROAD NAME AND DESIGN SPEED SHALL BE SHOWN ON PLANS.
6. TRANSITION FROM PAVED SHOULDER TO EXISTING PAVEMENT EDGE SHALL BE 30:1 MIN.
7. ANY PAVEMENT WIDENING, TURNOUTS AND STREET IMPROVEMENTS WITHIN DISTANCE (X) (SEE TABLE 1, SHEET 3), SHALL BE SHOWN ON PLANS.
8. 2' MIN. PAVED SHOULDERS SHALL BE USED ON NON-PAN COLLECTOR AND NON-THOROUGHFARE ROADWAYS.
4' MIN. PAVED SHOULDERS SHALL BE USED ON THOROUGHFARE ROADWAYS UNLESS OTHERWISE SHOWN IN ROADWAY PRODUCTION TYPICAL SECTIONS.
9. IF THE PROPOSED TRANSITION FALLS WITHIN DISTANCE (X) (SEE TABLE 1, SHEET 3), OF AN EXISTING TURN LANE TRANSITION, THEN THE AREA BETWEEN TRANSITIONS SHALL BE CONSTRUCTED AS A CONTINUOUS WIDENED SECTION, FOR GEOMETRIC CONTINUITY.
10. VARIATIONS FROM THIS SET OF STANDARD DRAWINGS SHALL BE APPROVED BY THE DIRECTOR OF TRAFFIC DIVISION.
11. DRAWINGS SHALL BE SUBMITTED EITHER IN 1"=20', 1"=30' OR 1"=40' SCALE.
12. THE FOLLOWING SYMBOL " " ON ANY SHEET IN THIS DOCUMENT, SHOWS TRAFFIC FLOW AND IS NOT A PAVEMENT MARKING.
13. 'O' OR FDP DENOTES FLEXIBLE DELINEATOR POST.
14. ALL REMOVAL OF PAVEMENT MESSAGES AND ARROWS SHALL BE IN BLOCK STYLE SUCH THAT THE MESSAGE IS NO LONGER DISCERNIBLE. METHOD OF REMOVAL SHALL BE APPROVED BY THE DIRECTOR OF TRAFFIC DIVISION.
15. WHEN TRAFFIC EXCEEDS 75 VEHICLES PER HOUR FOR RIGHT TURNS OR 30 VEHICLES PER HOUR FOR LEFT TURNS, THE TURN LANES SHALL BE CONSTRUCTED AS PER TABLES 3 & 4 ON SHEET 3 OF 12.
16. FOR SIGN SIZES REFER TO TABLES 2B-1, 2C-1 & 2C-2 OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. ALL SIGNS SHALL BE DIAMOND GRADE REFLECTIVE SHEETING.
17. TEMPORARY PAVEMENT MARKINGS SHALL BE APPLIED TO THE INTERMEDIATE ASPHALT COURSE AND SHALL CONSIST OF FOIL-BACKED TAPE OR PAINT. TEMPORARY PAVEMENT MARKINGS APPLIED TO THE FINAL ASPHALT COURSE SHALL CONSIST OF FOIL-BACKED TAPE.
18. TRANSITION RATES AT THE INTERSECTION APPROACHES SHALL BE:
 - 30:1 FOR < 40 MPH
 - 45:1 FOR 45 MPH
19. ALL PAVEMENT MARKINGS SHALL BE INSTALLED OR REPLACED ON EACH APPROACH WITH THE SAME MATERIAL USED AT THE TRANSITION, CONSTRUCTED FOR A DISTANCE (X), (SEE TABLE 1, SHEET 3) TO CONFORM WITH EXISTING ROADWAY DESIGN AS APPROVED BY THE DIRECTOR OF TRAFFIC DIVISION.
20. PAVEMENT MARKING INSTALLATIONS SHALL BE IN ACCORDANCE WITH THE FLORIDA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE MUTCD & FDOT DESIGN MANUAL + FDOT STANDARD PLANS (LATEST EDITIONS), HOWEVER, WHERE CONFLICTS EXIST, THIS GUIDE (T-P-24) SHALL TAKE PRECEDENT OVER FDOT STANDARD SPECIFICATIONS.
21. IF EXISTING PAVEMENT MARKING MATERIAL IS NOT COMPATIBLE WITH ALKYD THERMOPLASTIC, IT SHALL BE REMOVED PRIOR TO COMMENCEMENT OF WORK.
22. DISTANCE BETWEEN THE STOP BAR AND CROSSWALK SHALL BE (MINIMUM) 4 FEET.
23. ALL PAVEMENT MARKINGS SHALL HAVE REFLECTIVITY OF NOT LESS THAN 300 MILLICANDELAS PER SQUARE METER PER LUX AT INSTALLATION.
24. ALL LONGITUDINAL PAVEMENT MARKINGS SHALL BE 6" WIDE, UNLESS OTHERWISE NOTED, PER FDOT ROADWAY AND TRAFFIC DESIGN STANDARDS.
25. ALL PAVEMENT MARKINGS SHALL BE SHOWN TO SCALE ON THE PLANS.
26. ALL PAVEMENT MARKING MATERIAL SHALL BE ON THE APPROVED FLORIDA DEPARTMENT OF TRANSPORTATION "APPROVED PRODUCTS LIST" (APL).
27. ADDITIONAL REQUIREMENTS FOR PAVEMENT MARKINGS AT SIGNALIZED INTERSECTIONS ARE SHOWN IN THE PALM BEACH COUNTY TRAFFIC SIGNAL INSTALLATION STANDARDS AND DETAILS (LATEST EDITION).

28. THERMOPLASTIC SHALL BE USED UNLESS OTHERWISE APPROVED BY THE DIRECTOR OF TRAFFIC DIVISION. OF TRANSPORTATION (F.D.O.T.) STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION. MINIMUM THICKNESS SHALL BE 90 MILS (ALKYD ONLY), EXTRUDED ONLY, ON ALL COUNTY MAINTAINED ROADWAYS.
29. THERMOPLASTIC SHALL BE USED UNLESS OTHERWISE APPROVED BY THE DIRECTOR OF TRAFFIC DIVISION.
30. THERMOPLASTIC SHALL NOT BE INSTALLED ON ROADWAY UNTIL FOURTEEN (14) CALENDAR DAYS AFTER THE FINAL LIFT OF ASPHALT HAS BEEN COMPLETED, UNLESS OTHERWISE SPECIFIED BY THE DIRECTOR OF TRAFFIC DIVISION.
31. ALL REFLECTIVE PAVEMENT MARKERS SHALL BE APPROVED BY THE DIRECTOR OF TRAFFIC DIVISION BEFORE INSTALLATION.
32. ALL MARKERS SHALL BE CLASS "B" AS PER FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE SECTION 706.
33. REFLECTORS SHALL BE PLACED AT EACH CROSS HATCH. SPACING VARIES WITH SPEED AS SHOWN IN THESE DETAILS (SEE SHEET 6).
34. REFLECTORS SHALL BE EQUALLY SPACED BETWEEN POINTS A AND B BUT NOT MORE THAN 12" APART (SEE SHEET 6).
35. EPOXY OR BITUMINOUS SHALL BE USED WHEN INSTALLING RPM'S ON CONCRETE OR ASPHALT PAVEMENT. THERMOPLASTIC MAY BE USED TO SET RPM'S ON ASPHALT PAVEMENT ONLY.
36. RPM'S SHALL BE PLACED
- (ONE) 1" TO THE LEFT OR RIGHT OF A SOLID THERMOPLASTIC STRIPE, OR
 - IN LINE WITH STRIPE, IF THERE ARE SKIPS.
37. PAVER STONE SHALL CONFORM WITH PALM BEACH COUNTY LAND DEVELOPMENT DESIGN STANDARDS:
- THE PAVER STONE SHALL BE PRE-APPROVED BY PALM BEACH COUNTY TRAFFIC ENGINEER PRIOR TO THE INSTALLATION.
 - FOR PRIVATE SUBDIVISIONS THAT ARE EXPECTED TO HAVE A TRAFFIC SIGNAL, NO PAVER STONE SHALL BE CONSTRUCTED 60' FROM RIGHT OF WAY ON OUTGOING LANE(S). INCOMING ROADWAY PAVER STONE MAY START AT THE RIGHT OF WAY LINE. BEFORE INSTALLATION OF THE TRAFFIC SIGNAL THE HOME OWNER ASSOCIATION WILL BE REQUIRED TO REMOVE THE PAVER STONES.
 - NO PAVER STONE SHALL BE CONSTRUCTED WITHIN PALM BEACH COUNTY RIGHT OF WAY UNLESS PERMITTED BY THE DIRECTOR OF TRAFFIC DIVISION AND THE OWNERS ENTER INTO A REMOVAL AGREEMENT WITH THE COUNTY (LAND DEVELOPMENT DIVISION). PAVER STONE WILL NOT BE PERMITTED FOR SIDEWALK CONSTRUCTION.
38. FOR BICYCLE MARKINGS SEE FLORIDA GREEN BOOK CHAPTER 9 AND THIS T-P-24 WITH THE FOLLOWING MODIFICATIONS.
- BICYCLE MARKINGS SHALL BE PROVIDED AT THE BEGINING OF BIKE LANES AND AT 1/4 MILE INTERVALS.
 - BICYCLE SIGNS SHALL BE INSTALLED AT THE BEGINING AND END OF BIKE LANES ONLY.
- IN LIEU OF R4-4 SIGNS THE COUNTY PREFERS R3-17.
-
- FOR STANDARD BUFFERED BIKELANE SEE FIGURE 9-17 OF GREEN BOOK.
 - BICYCLE MARKINGS SHALL BE PROVIDED AT ALL SIGNALIZED INTERSECTIONS PER FLORIDA GREEN BOOK.
39. ALL RAILROAD GRADE CROSSINGS SHALL HAVE DYNAMIC ENVELOPE PAVEMENT MARKINGS PER FDOT INDEX 509-070.
40. FOR PAVEMENT MARKINGS AT RAILROAD CROSSINGS, FOLLOW THE FLORIDA DESIGN MANUAL CHAPTER 220 WITH THE FOLLOWING EXCEPTIONS:
- REPLACE ALL SKIP LANE LINES WITH SOLID LINES FOR THE FOLLOWING DISTANCE: FROM STOP BAR TO STOP BAR OF EACH APPROACH, THEN UPSTREAM FOR A DISTANCE "A" PLUS 15 FEET. FOR DISTANCE "A" SEE TABLE IN EXHIBIT 220-1.
 - PLACE RPMs AT 10' MAXIMUM ON CENTER FOR THE FOLLOWING DISTANCE: FROM STOP BAR TO STOP BAR OF EACH APPROACH EXCLUDING THE FOUL AREA, THEN UPSTREAM FOR A DISTANCE "A" PLUS 15 FEET. FOR DISTANCE "A", SEE TABLE IN EXHIBIT 220-1.
41. DYNAMIC ENVELOPE IS TO FOLLOW INDEX 711-001. PLEASE NOTE THAT THE EDGE LINES ARE TO NOT CONTINUE THROUGH THE DYNAMIC ENVELOPE AS SHOWN IN INDEX 711-001. THEN LANE LINES ARE TO GO THROUGH THE DYNAMIC ENVELOPE MARKING AS SHOWN IN THE FLORIDA DESIGN MANUAL.
42. WHEN EMPLOYING THE "DO NOT BLOCK" INTERSECTION MARKINGS, THE COUNTY PREFERS THE MUTCD, FIGURE 3B-24 OPTION "B".
43. BIKE LANES CONSIST OF THE FOLLOWING:
- MIN OF FIVE (5) FT WIDE ASPHALT,
 - MIN OF FOUR (4) FT WIDE ASPHALT WITH A TWO (2) FT CURB AND GUTTER (I.E. TYPE F OR VALLEY GUTTER), OR
 - MIN OF FOUR (4) FT WIDE ASPHALT WITH A ONE (1) FT SODDED, STABILIZED SHOULDER (6% OR LESS CROSS SLOPE).



LEGEND:
E.O.P = EDGE OF PAVEMENT

TABLE #1

LENGTHS OF APPROACH STRIPING (FT.)

ALL PAVEMENT MARKING & R.P.M.'S SHALL BE REDONE OR BROUGHT UP TO PRESENT STANDARDS.

SPEED	DISTANCE (X) (FT.)
30	500
35	550
40	600
45	700
50	800

TABLE #2

DISTANCE BETWEEN CROSS HATCHED LINES

POSTED SPEED LIMIT (M.P.H.)	DISTANCE (Z) (FT.)
≤ 30	10
≤ 40	20
45	30
≥ 50	40

TABLE #3

SINGLE TURN LANE STORAGE (MIN. - FT)

		TURNING VEHICLES PER HOUR									
		30-60	61-90	91-120	121-150	151-180	181-210	211-240	241-270	271-300	>301
MPH	55	365	390	415	440	465	490	515	540	565	590
	50	300	325	350	375	400	425	450	475	500	525
	45	280	280	295	320	345	370	395	420	445	470
	40	280	280	280	280	285	310	335	360	385	410
	35	280	280	280	280	280	290	315	340	365	390

TABLE 3 NOTES:

1. STORAGES SHOWN ARE MINIMUMS IN FEET, EXCLUDING 50' TAPER.
2. APPLIES TO RIGHT OR LEFT TURN LANES, SIGNALIZED OR UNSIGNALIZED.
3. ENTRY SPEED ASSUMED TO BE 5 MPH LESS THAN SPEED LIMIT; STORAGE LENGTHS CALCULATED UTILIZING FLORIDA DESIGN MANUAL SECTION 212.
4. ASSUMES UNIFORM ARRIVALS WITH TOTAL QUEUE DISPERSAL EVERY 2 MINUTES.
5. 6'-10' WHITE SKIP LINE LENGTH - 180'.
6. USE OF SINGLE LEFT TURN LANE FOR VOLUMES IN EXCESS OF 300 REQUIRES APPROVAL FROM THE DIRECTOR OF TRAFFIC DIVISION.

TABLE #4

DUAL TURN LANE STORAGE (MIN. - FT)

		TURNING VEHICLES PER HOUR						
		<240	241-300	301-360	361-420	421-480	481-540	<541
MPH	55	490	515	540	565	590	615	665
	50	450	450	475	500	525	550	600
	45	450	450	450	450	470	495	545
	40	450	450	450	450	450	450	485
	35	450	450	450	450	450	450	465

TABLE 4 NOTES:

1. STORAGES SHOWN ARE MINIMUMS IN FEET, EXCLUDING 100' TAPER.
2. APPLIES TO RIGHT OR LEFT TURN LANES.
3. ENTRY SPEED ASSUMED TO BE 5 MPH LESS THAN SPEED LIMIT; STORAGE LENGTHS CALCULATED UTILIZING FLORIDA DESIGN MANUAL SECTION 212.
4. ASSUMES UNIFORM ARRIVALS WITH TOTAL QUEUE DISPERSAL EVERY 2 MINUTES AT 95% LEVEL OF CONFIDENCE.
5. ASSUME 50% - 50% LANE DISTRIBUTION.
6. 6'-10' WHITE SKIP LINE LENGTH - 250'

STRIPING KEY

A = 6" SOLID WHITE
B = 8" SOLID WHITE
C = 12" SOLID WHITE
D = 18" SOLID WHITE
E = 24" SOLID WHITE
F = 6" SKIP WHITE TYP (10'-30')
G = 6" SKIP WHITE TYP (6'-10')
H = 6" SKIP WHITE TYP (2'-4')
I = 6" SOLID YELLOW
J = 18" SOLID YELLOW
K = 6" DOUBLE YELLOW
L = 6" SKIP YELLOW TYP (10'-30')
M = 6" SKIP YELLOW TYP (6'-10')
N = 6" SKIP YELLOW TYP (2'-4')
P = RPM BI-DIRECTIONAL AMBER/AMBER
R = FDP WHITE
S = FDP YELLOW
T = RPM BI-DIRECTIONAL WHITE/RED
U = RPM BI-DIRECTIONAL RED/YELLOW

PALM BEACH COUNTY

ENGINEERING & PUBLIC WORKS

TRAFFIC DIVISION

DATE	
BY	
REVISION	
NO.	

SCALE:	NTS
APPROVED:	
DRAWN:	A.K.
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DATE:	7/15/24

TURN LANE GEOMETRICS
AND APPROACH STRIPING

PROJECT:

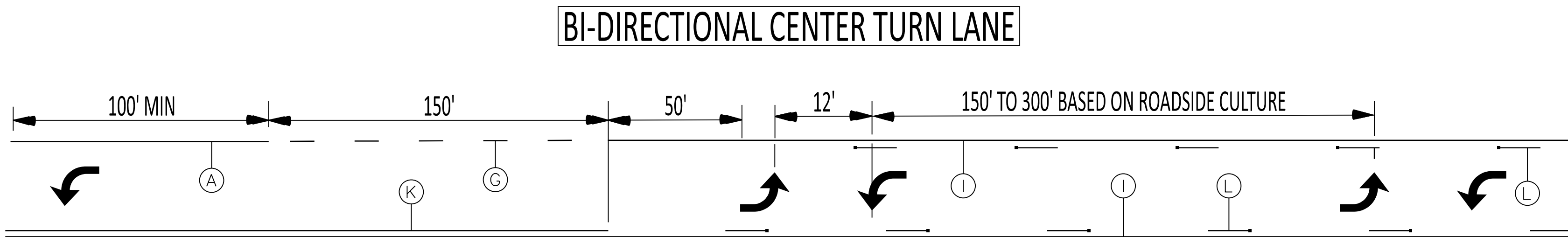
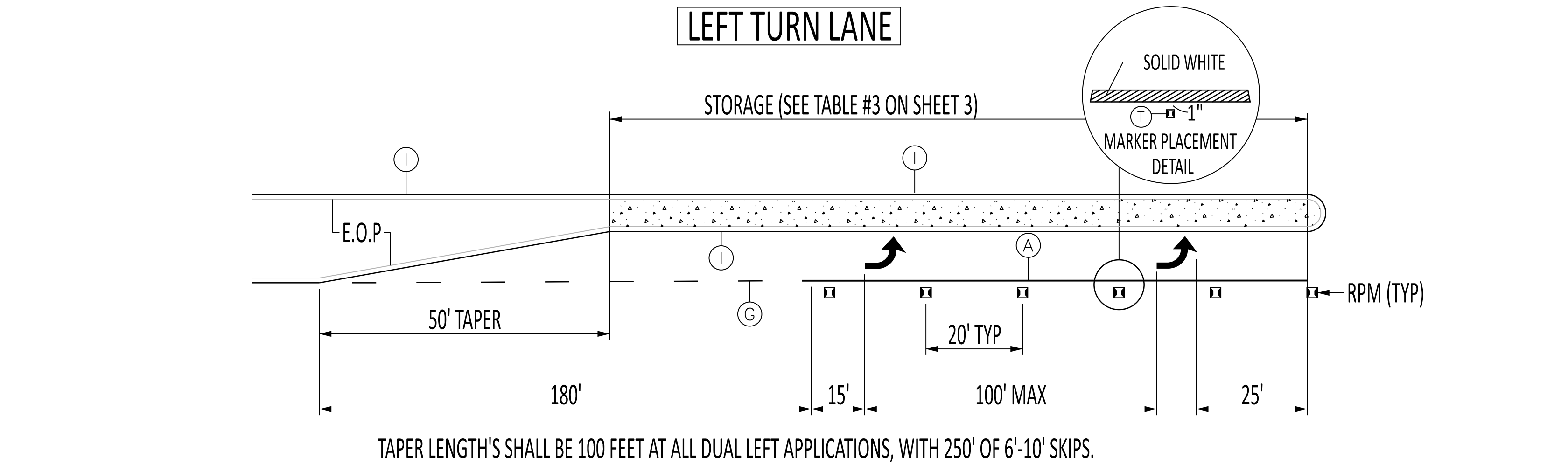
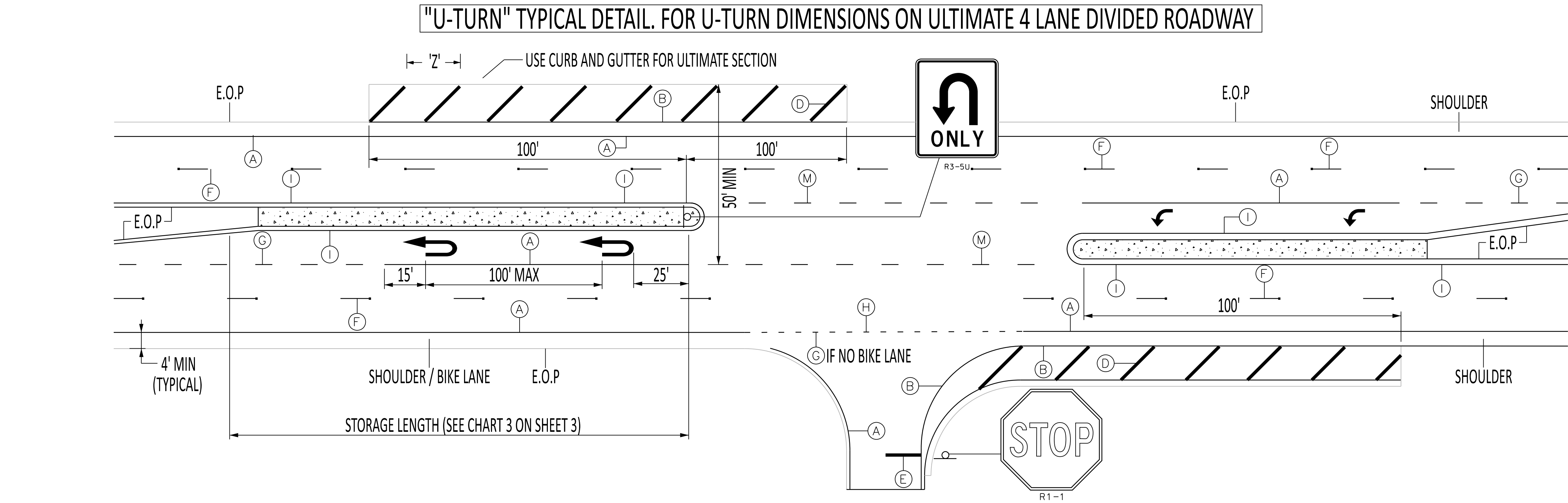
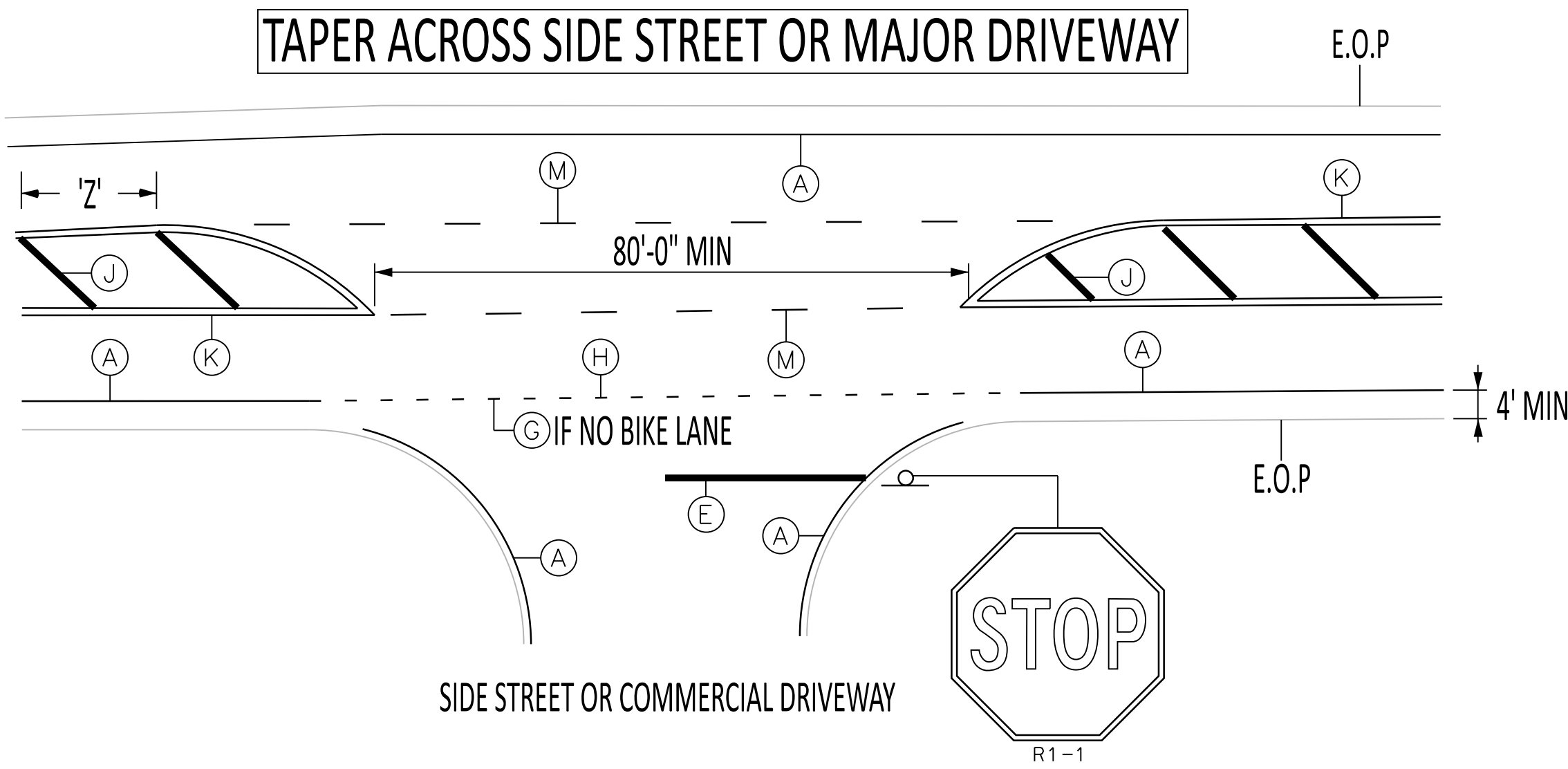
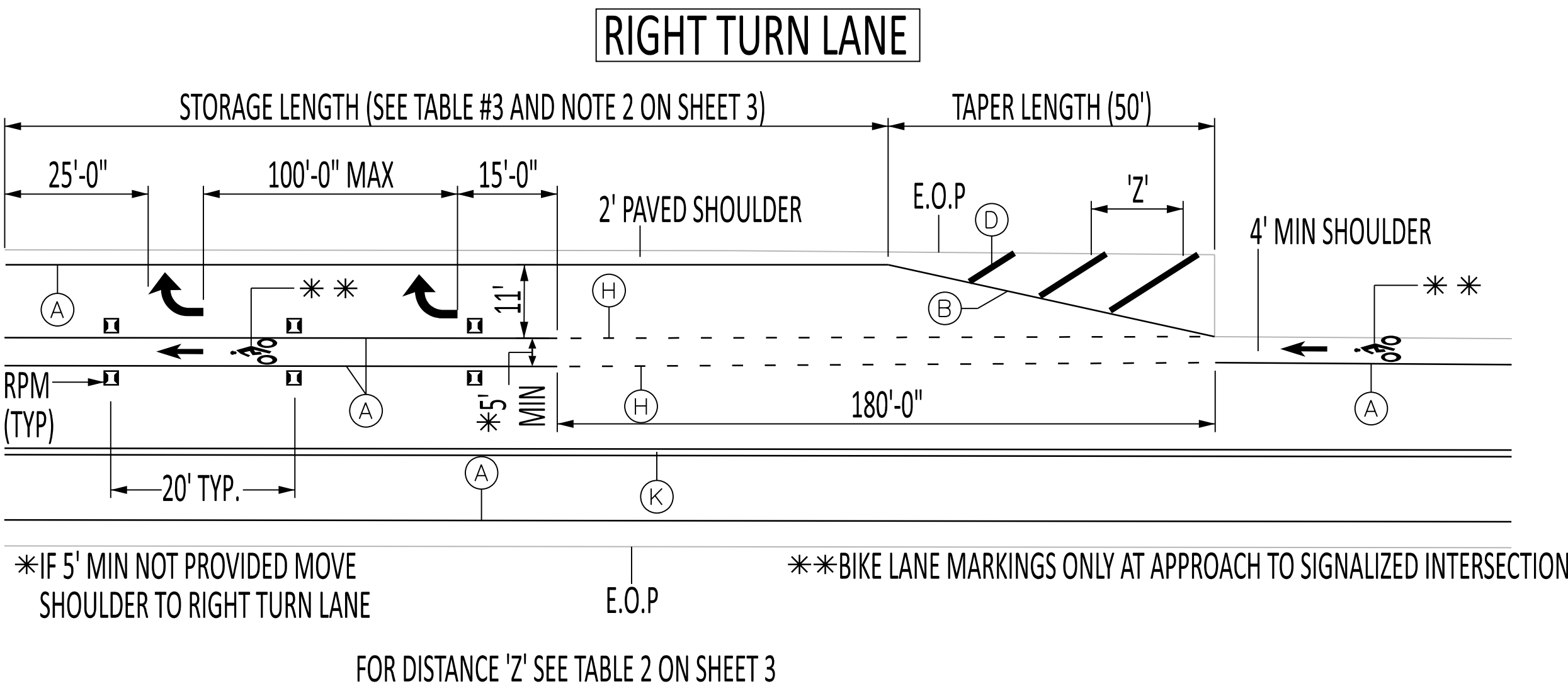
SHEET:

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OF:

12

T-P-24



- STRIPING KEY**
- A = 6" SOLID WHITE
 - B = 8" SOLID WHITE
 - C = 12" SOLID WHITE
 - D = 18" SOLID WHITE
 - E = 24" SOLID WHITE
 - F = 6" SKIP WHITE TYP (10'-30')
 - G = 6" SKIP WHITE TYP (6'-10')
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 - K = 6" DOUBLE YELLOW
 - L = 6" SKIP YELLOW TYP (10'-30')
 - M = 6" SKIP YELLOW TYP (6'-10')
 - N = 6" SKIP YELLOW TYP (2'-4')
 - P = RPM BI-DIRECTIONAL AMBER/AMBER
 - R = FDP WHITE
 - S = FDP YELLOW
 - T = RPM BI-DIRECTIONAL WHITE/RED
 - U = RPM BI-DIRECTIONAL RED/YELLOW

PALM BEACH COUNTY

ENGINEERING & PUBLIC WORKS

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NO.	DATE
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SCALE: NTS

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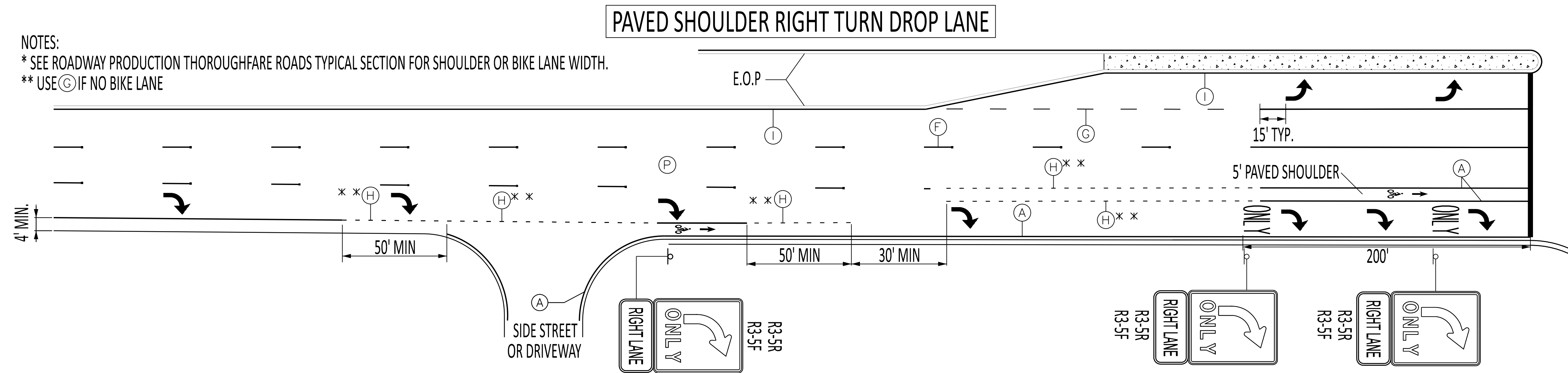
OTHER LANE DETAILS WITH PAVED SHOULDER

SHEET: 4

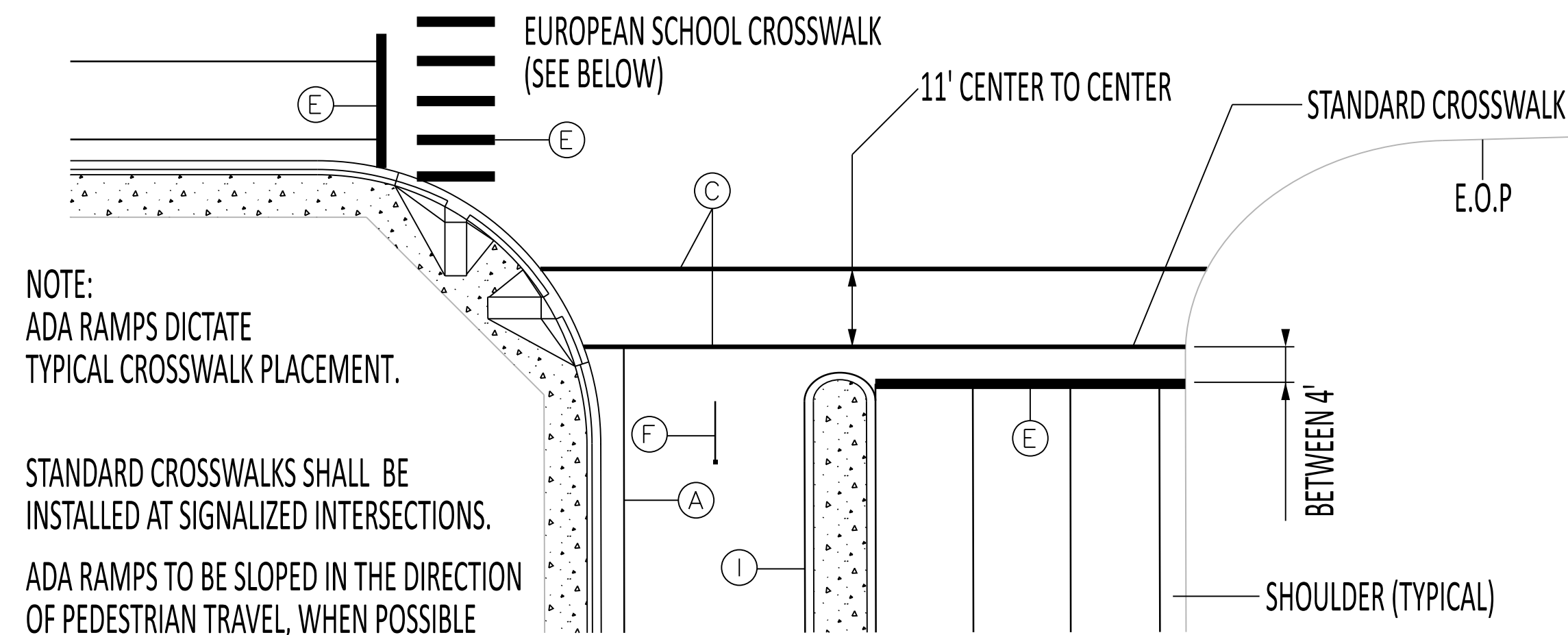
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T-P-24

NOTES:
* SEE ROADWAY PRODUCTION THOROUGHFARE ROADS TYPICAL SECTION FOR SHOULDER OR BIKE LANE WIDTH.
** USE ☉ IF NO BIKE LANE



EUROPEAN CROSSWALK & STOP BAR PLACEMENT

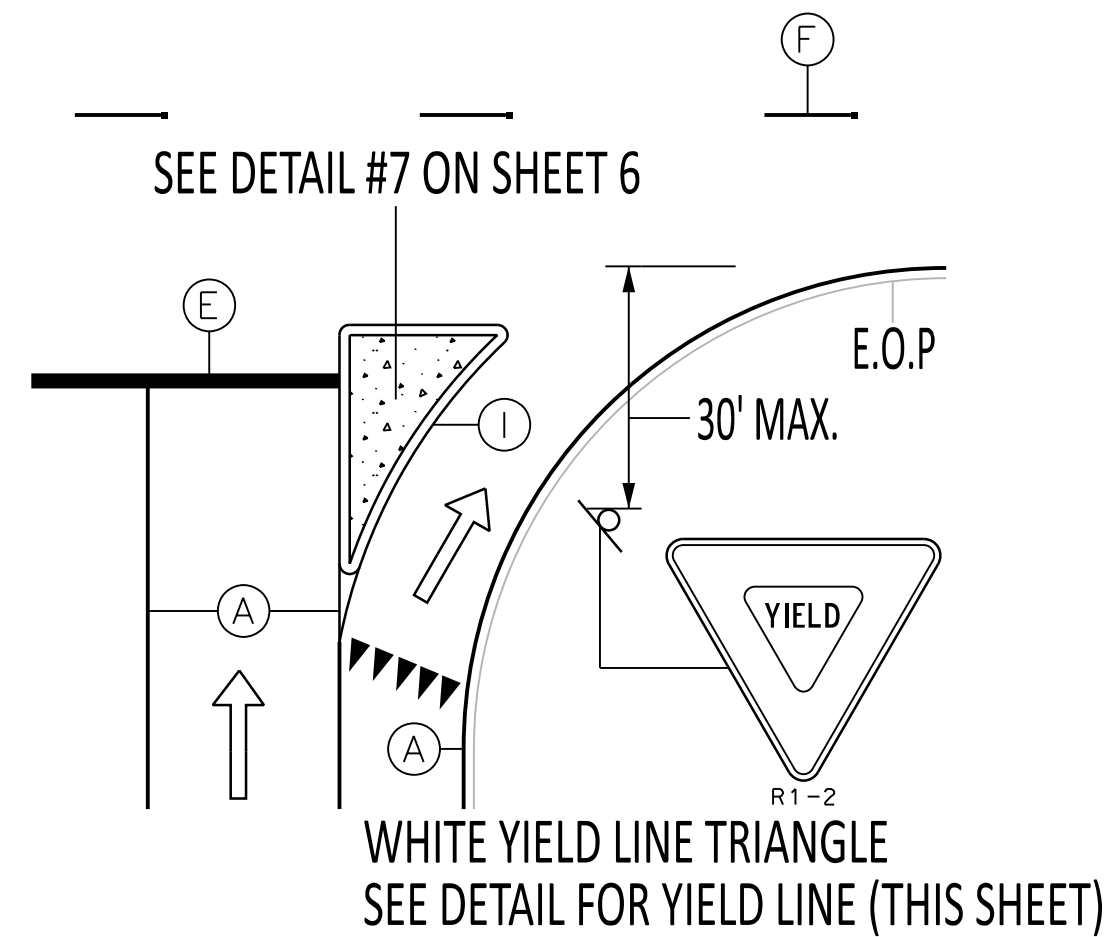


NOTE:
ADA RAMP SLOPES DICTATE
TYPICAL CROSSWALK PLACEMENT.

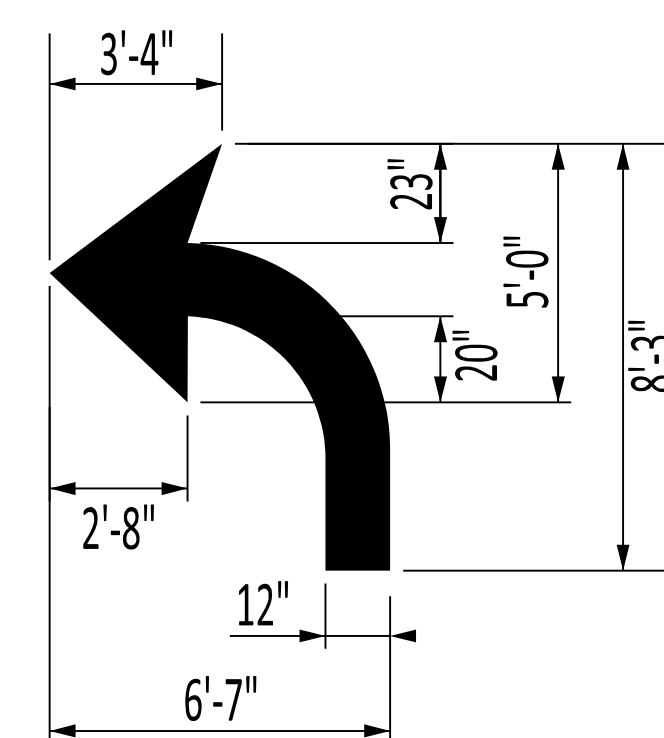
STANDARD CROSSWALKS SHALL BE
INSTALLED AT SIGNALIZED INTERSECTIONS.

ADA RAMP SLOPES TO BE SLOPED IN THE DIRECTION
OF PEDESTRIAN TRAVEL, WHEN POSSIBLE

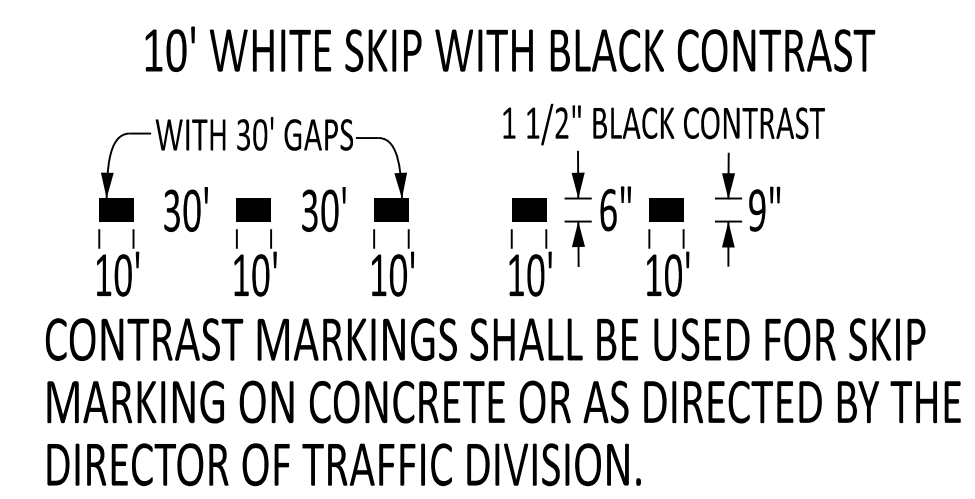
YIELD SIGN PLACEMENT



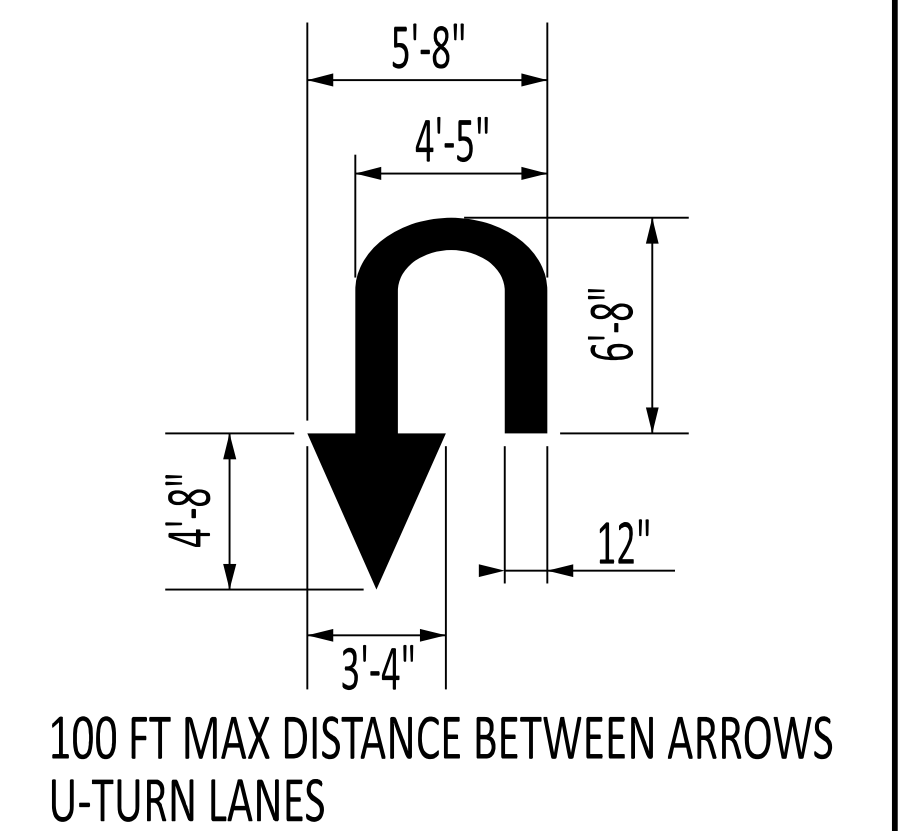
TYPICAL TURN LANE ARROW



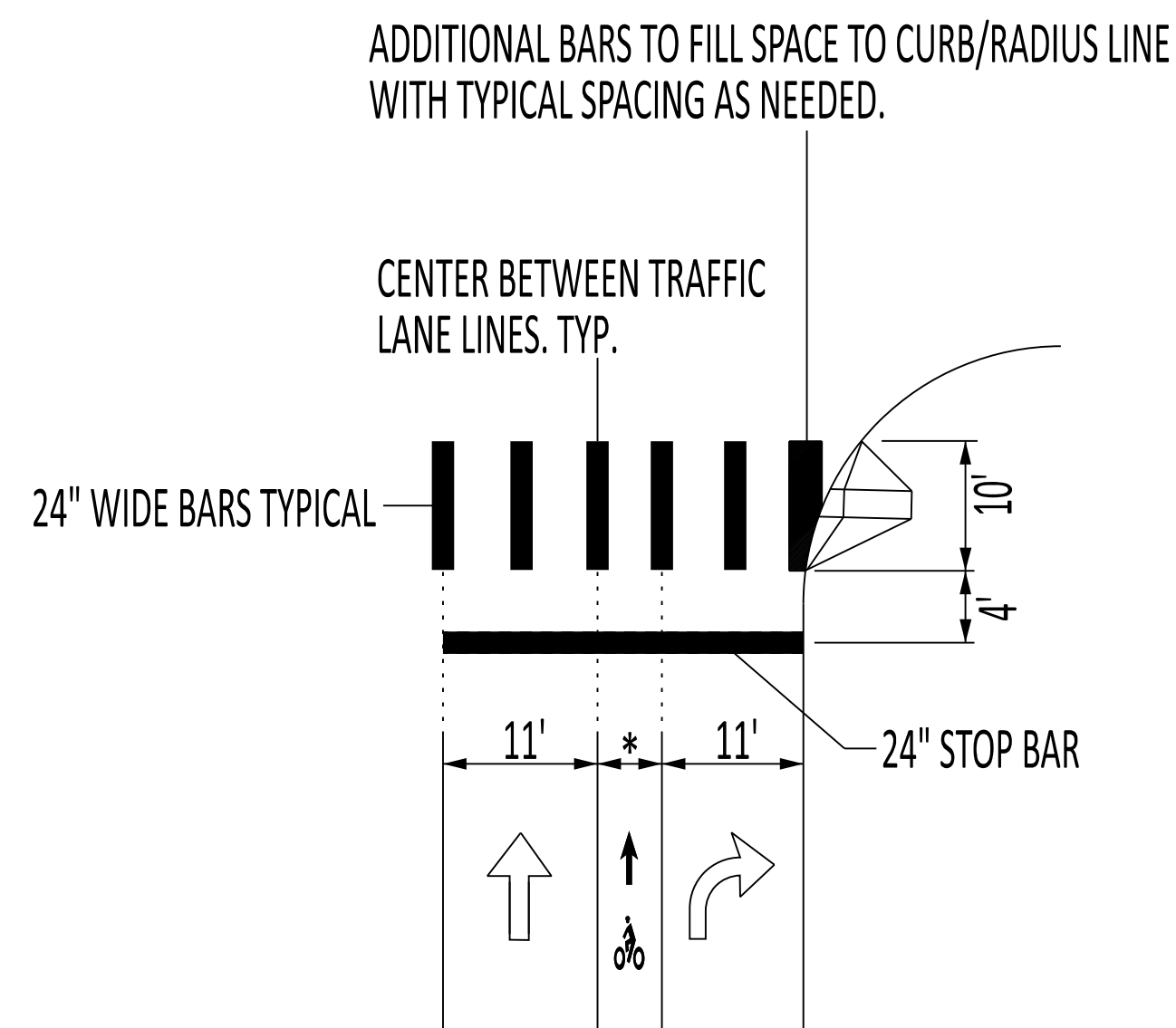
CONTRAST MARKINGS



TYPICAL U-TURN ARROW

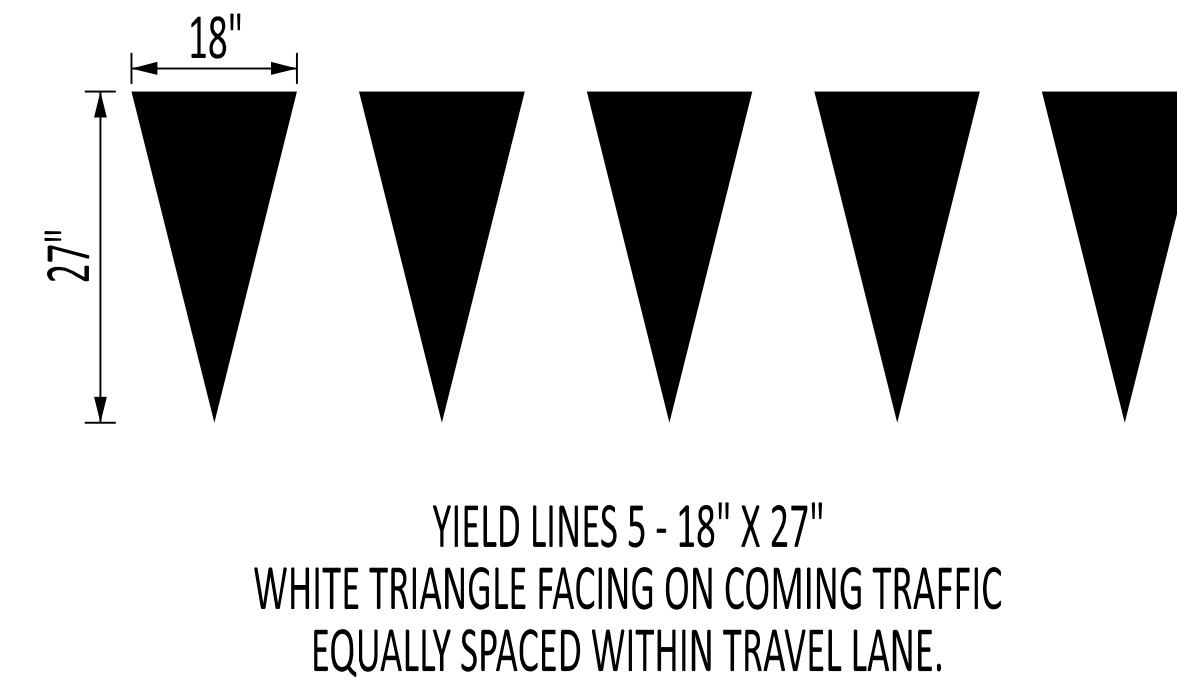


**EUROPEAN CROSSWALK
(FOR USE AT GUARDED SCHOOL INTERSECTIONS OR MID BLOCK CROSSINGS)**



* VARIES - USE ROADWAY PRODUCTION THOROUGHFARE
ROADS TYPICAL SECTION FOR DETAIL.
5' MIN. FOR BIKE LANE WIDTH.

DETAIL FOR YIELD LINE

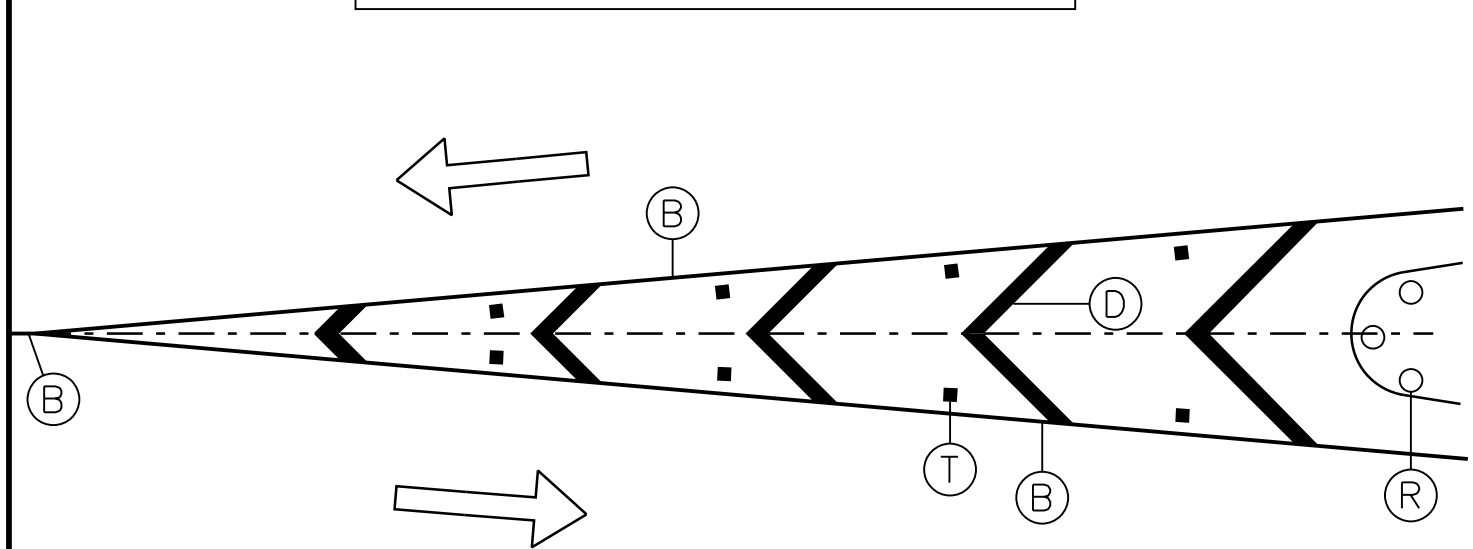


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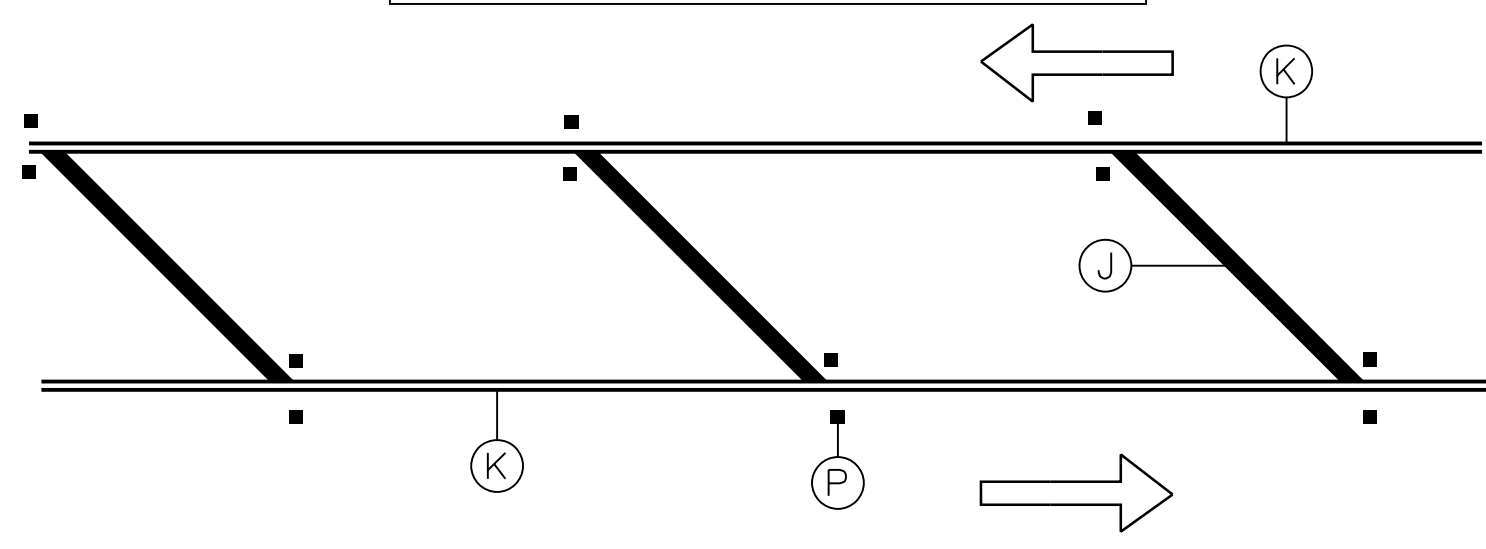
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APPROVED: A.K.
DRAWN: A.K.
CHECKED: 7/15/24
DATE: 7/15/24

OTHER LANE DETAILS

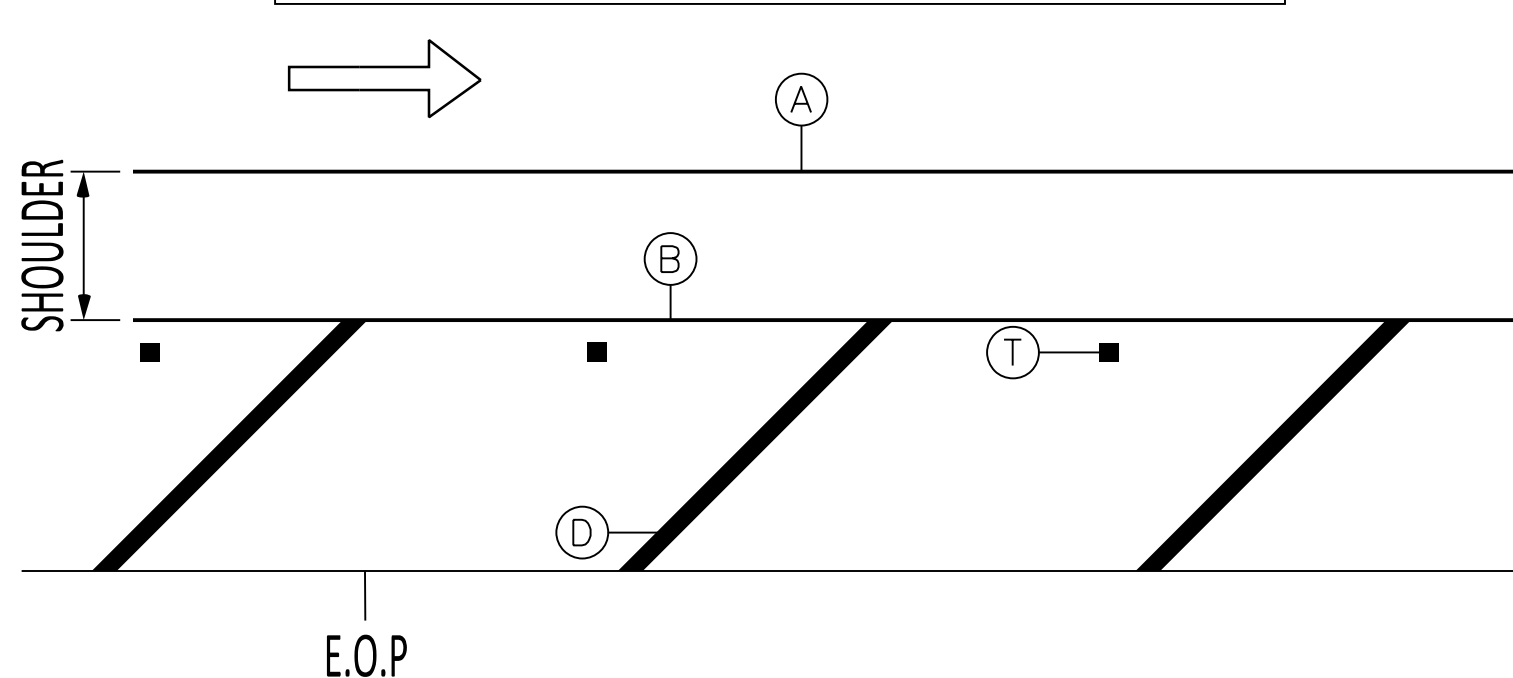
#1 THERMOPLASTIC GORE



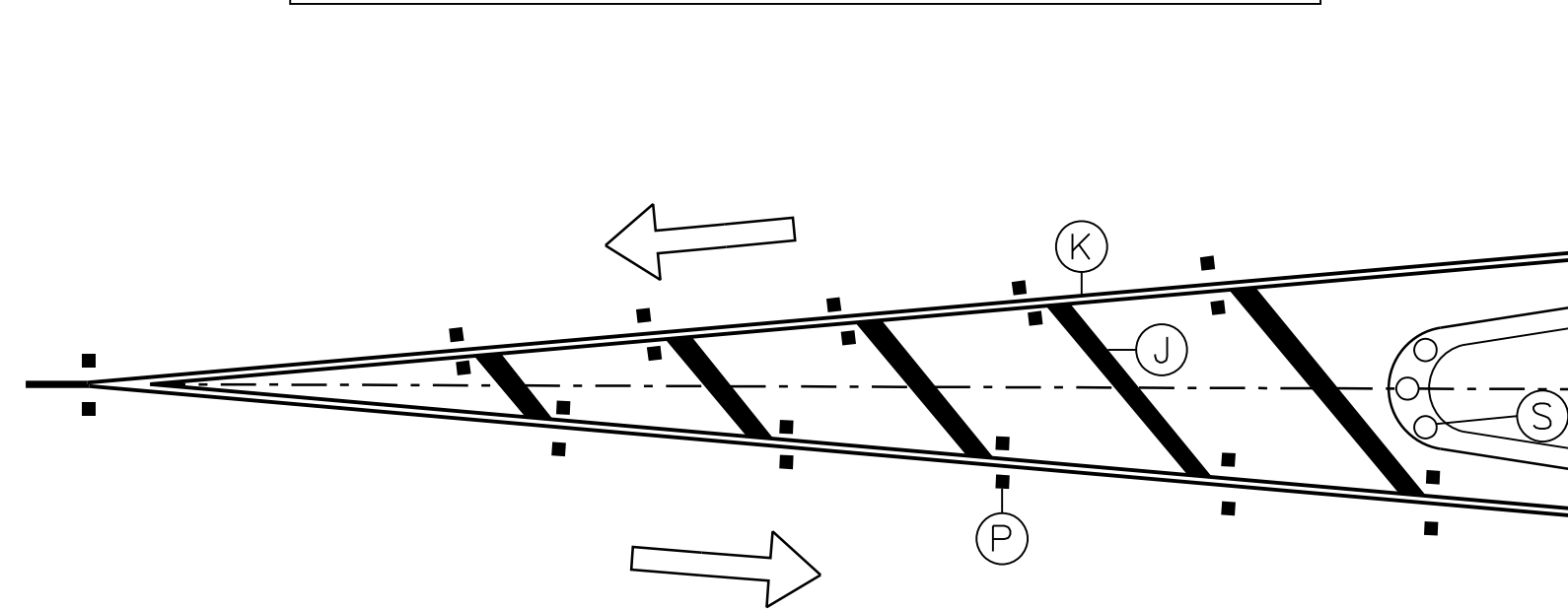
#2 THERMOPLASTIC ISLAND



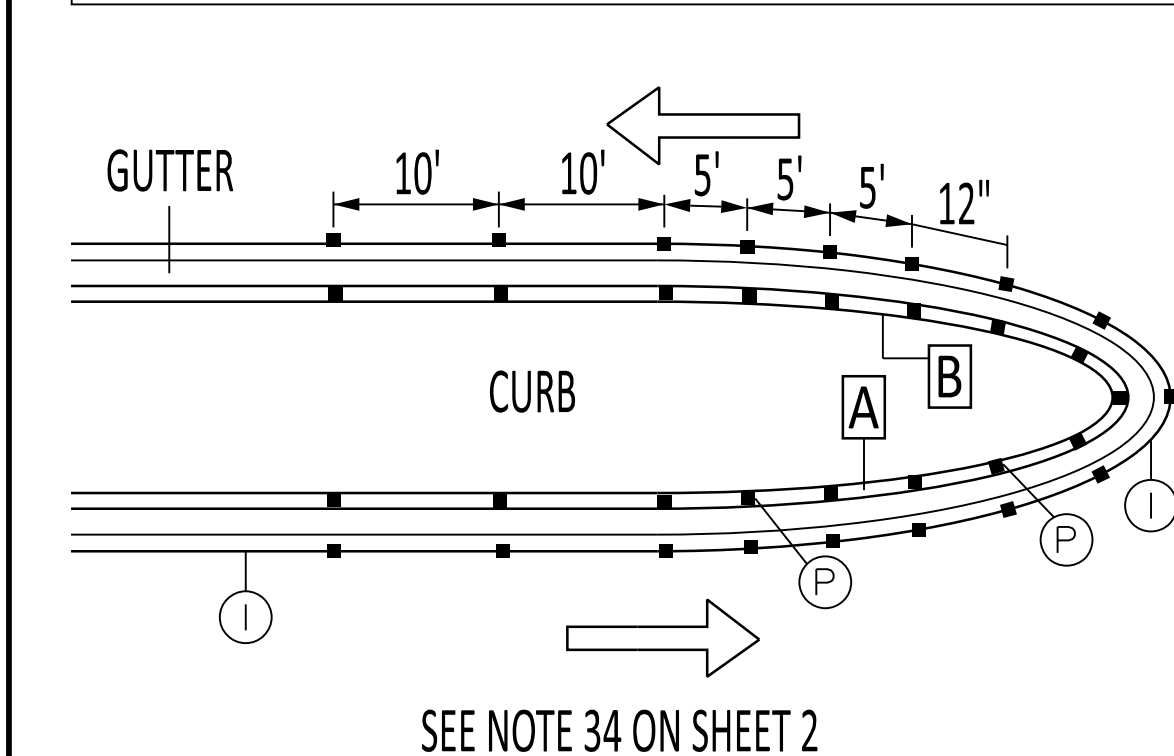
#3 EDGE OF PAVEMENT CROSSHATCH



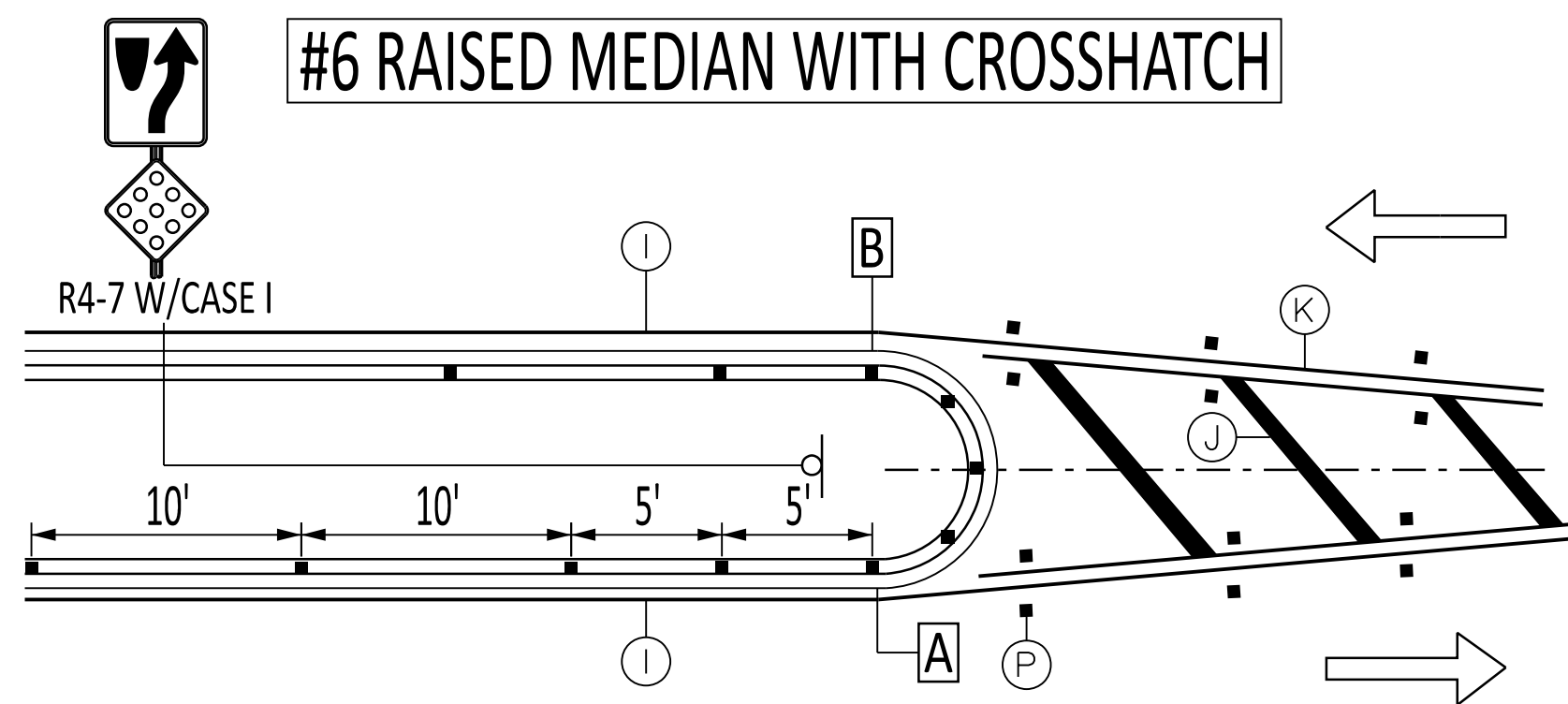
#4 GRASS MEDIAN WITH CROSSHATCH



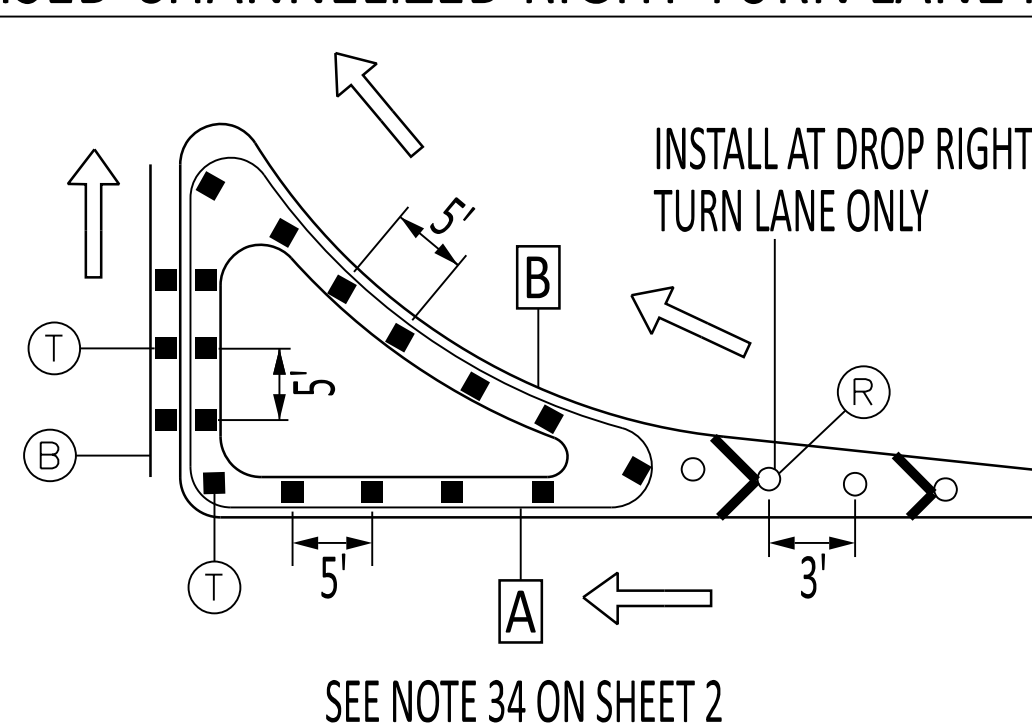
#5 RAISED MEDIAN WITH TAPERED NOSE



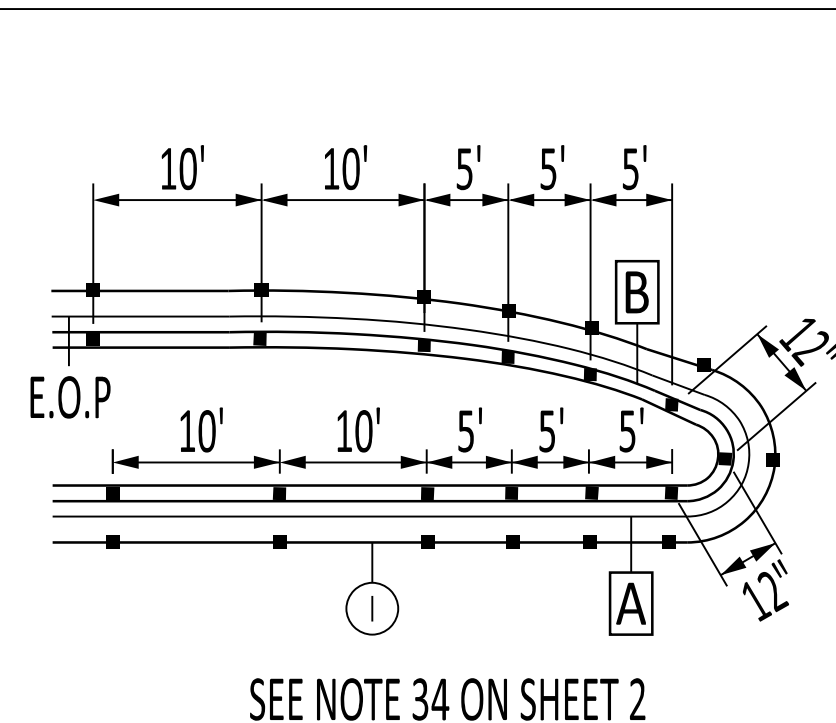
#6 RAISED MEDIAN WITH CROSSHATCH



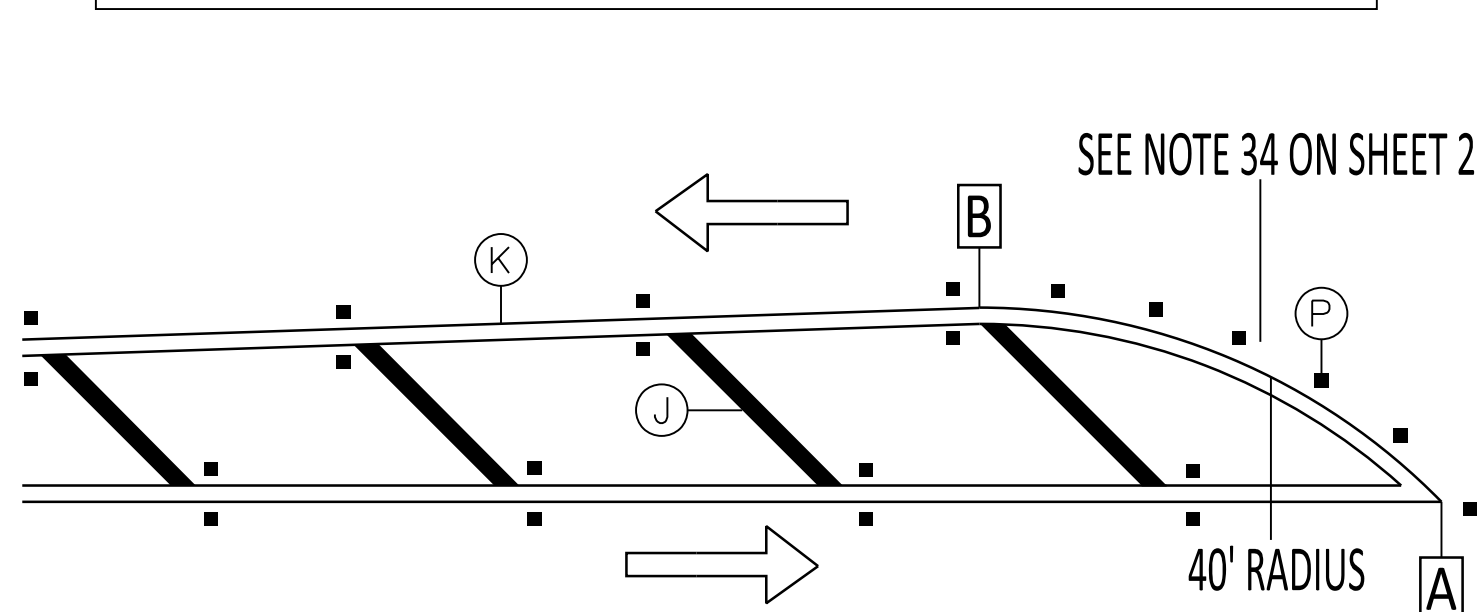
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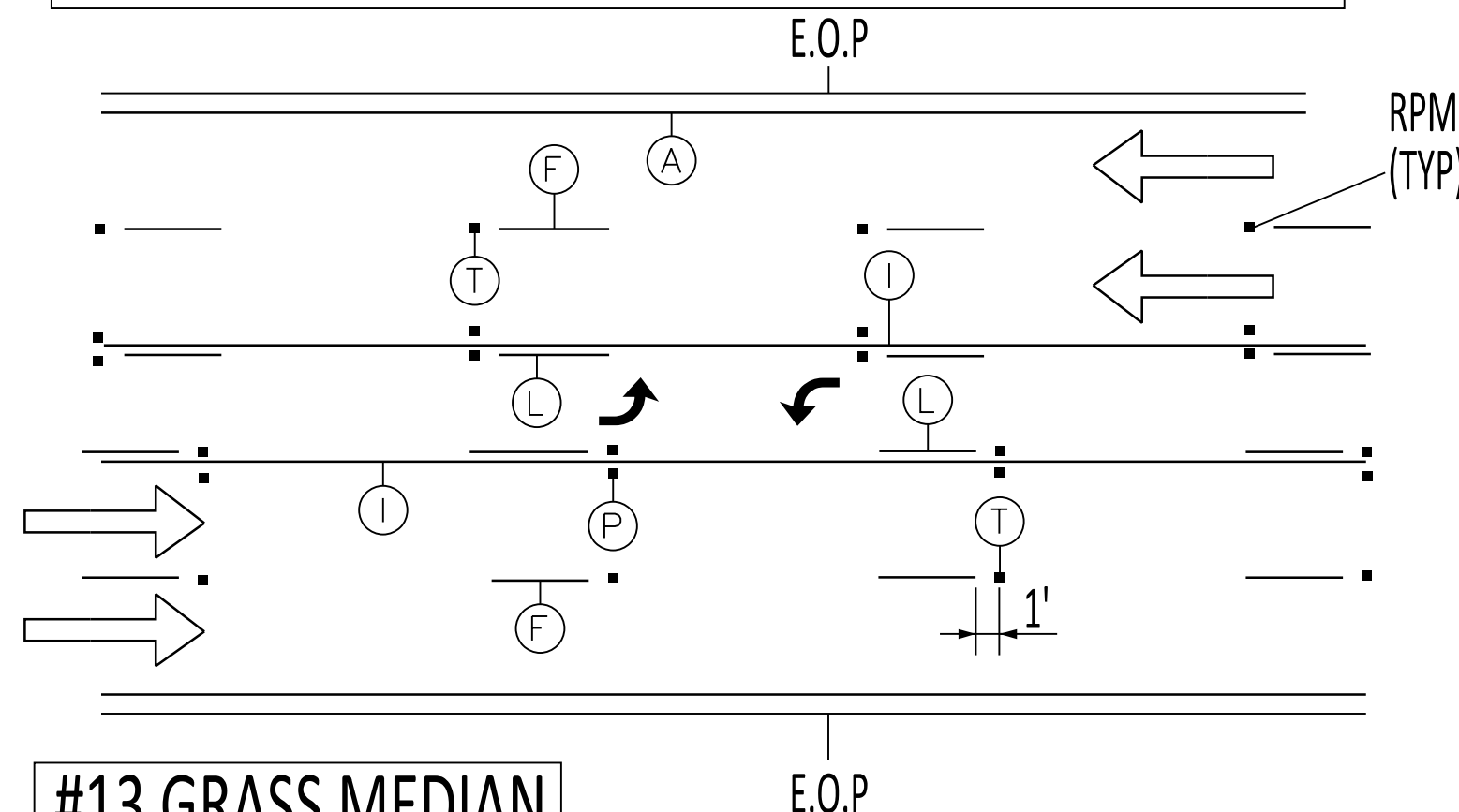
#8 RAISED MEDIAN WITH TAPERED NOSE



#9 THERMOPLASTIC MEDIAN AT INTERSECTION

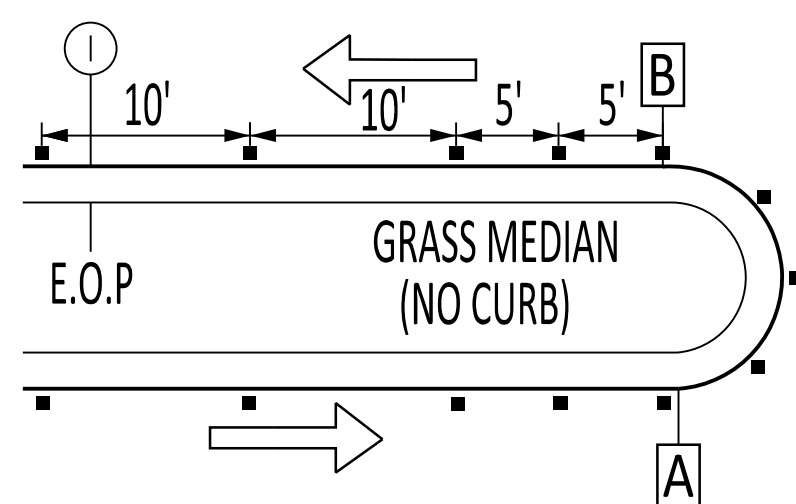


#10 SKIP LINE WITH BI-DIRECTIONAL CENTER TURN

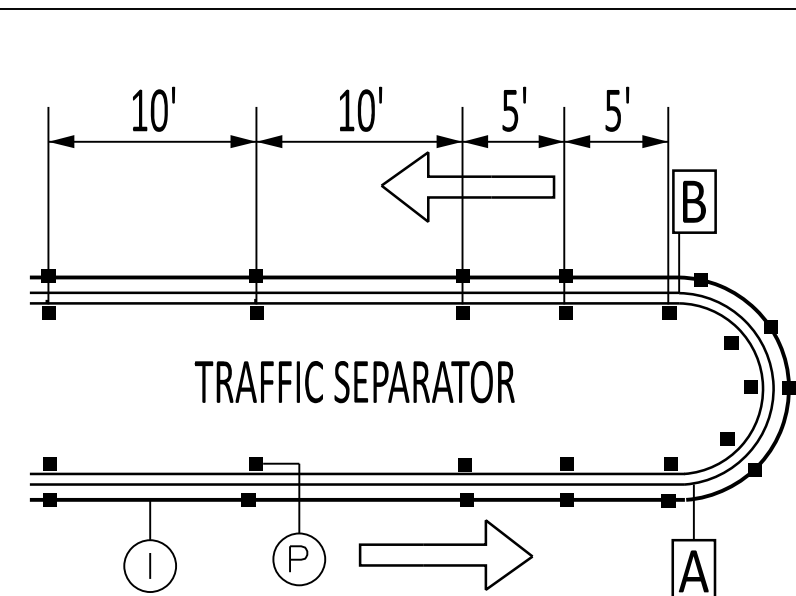


#13 GRASS MEDIAN

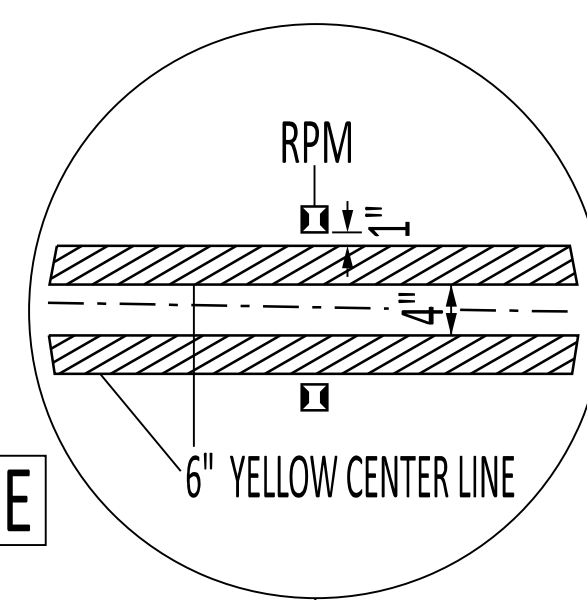
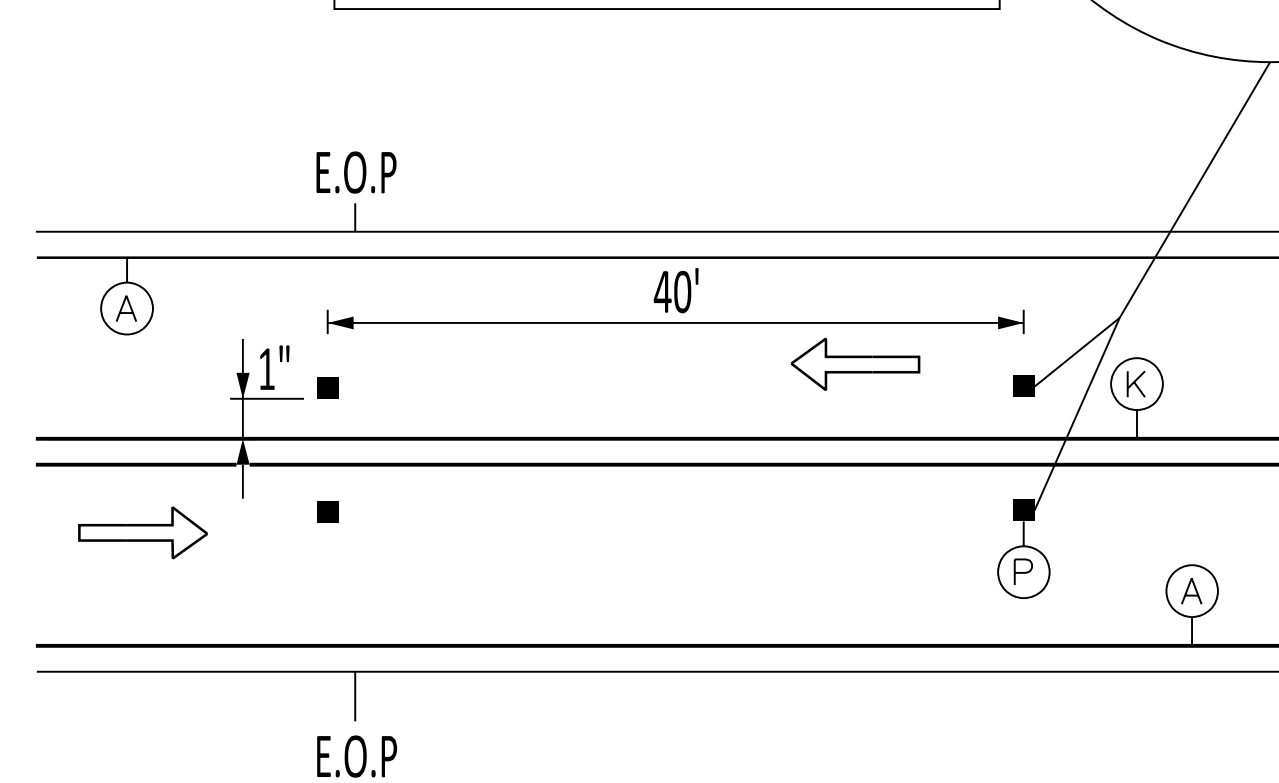
(SEE NOTE 34 ON SHEET 2)



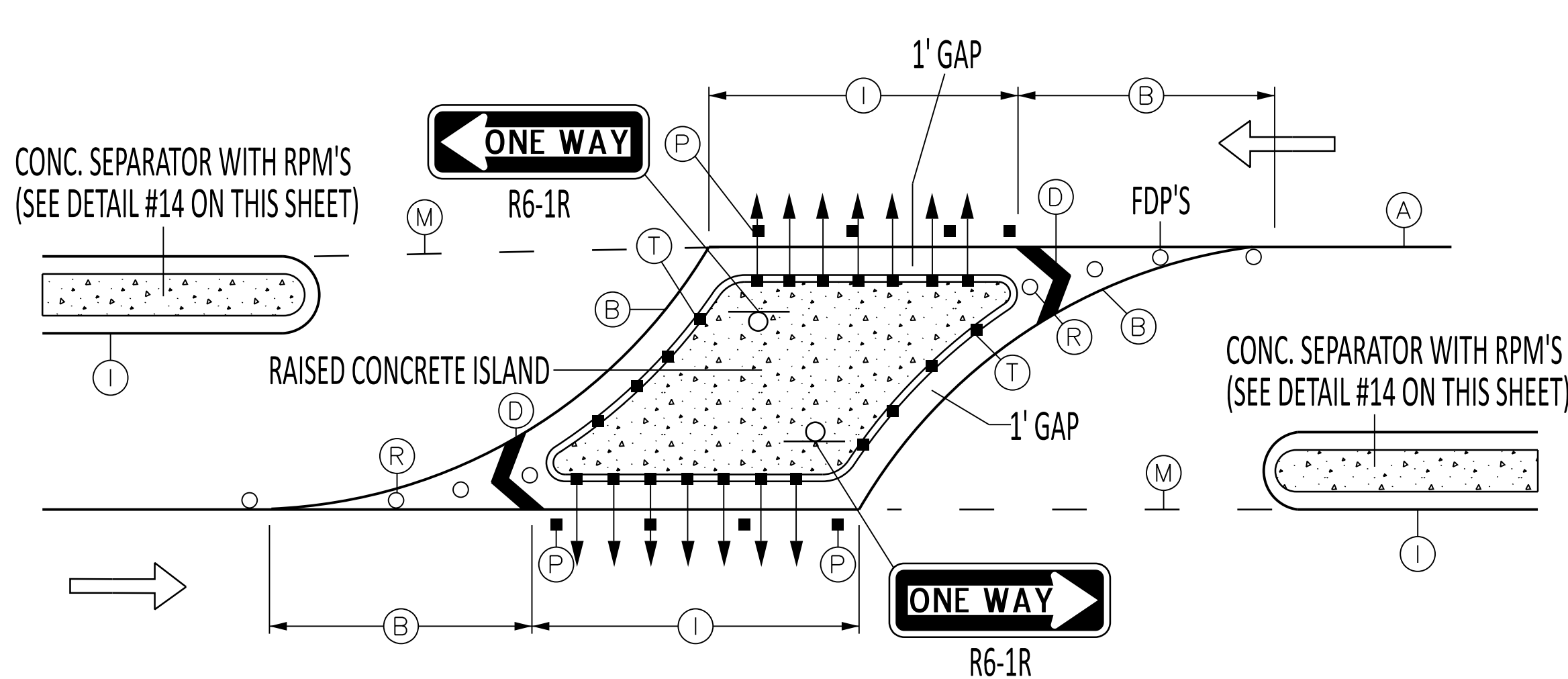
#14 RAISED CONCRETE TRAFFIC SEPARATOR



#11 DOUBLE SOLID LINE



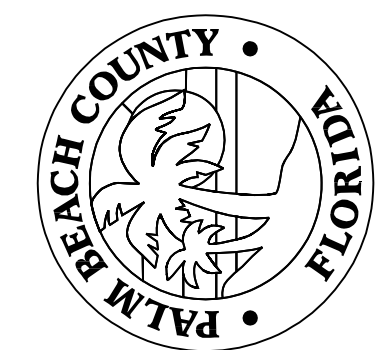
#12 RAISED MEDIAN - CHANNELIZED LEFT TURN/DIVIDED ROADWAY



NOTE:
↑ DENOTES DIRECTION OF FACE OF REFLECTIVE PAVEMENT MARKER.
↓ PAVEMENT MARKERS SHALL BE PLACED ON TOP OF ALL SIDES OF CONCRETE ISLAND 12\"/>

STRIPING KEY

- A = 6" SOLID WHITE
- B = 8" SOLID WHITE
- C = 12" SOLID WHITE
- D = 18" SOLID WHITE
- E = 24" SOLID WHITE
- F = 6" SKIP WHITE TYP (10'-30')
- G = 6" SKIP WHITE TYP (6'-10')
- H = 6" SKIP WHITE TYP (2'-4')
- I = 6" SOLID YELLOW
- J = 18" SOLID YELLOW
- K = 6" DOUBLE YELLOW
- L = 6" SKIP YELLOW TYP (10'-30')
- M = 6" SKIP YELLOW TYP (6'-10')
- N = 6" SKIP YELLOW TYP (2'-4')
- P = RPM BI-DIRECTIONAL AMBER/AMBER
- R = FDP WHITE
- S = FDP YELLOW
- T = RPM BI-DIRECTIONAL WHITE/RED
- U = RPM BI-DIRECTIONAL RED/YELLOW

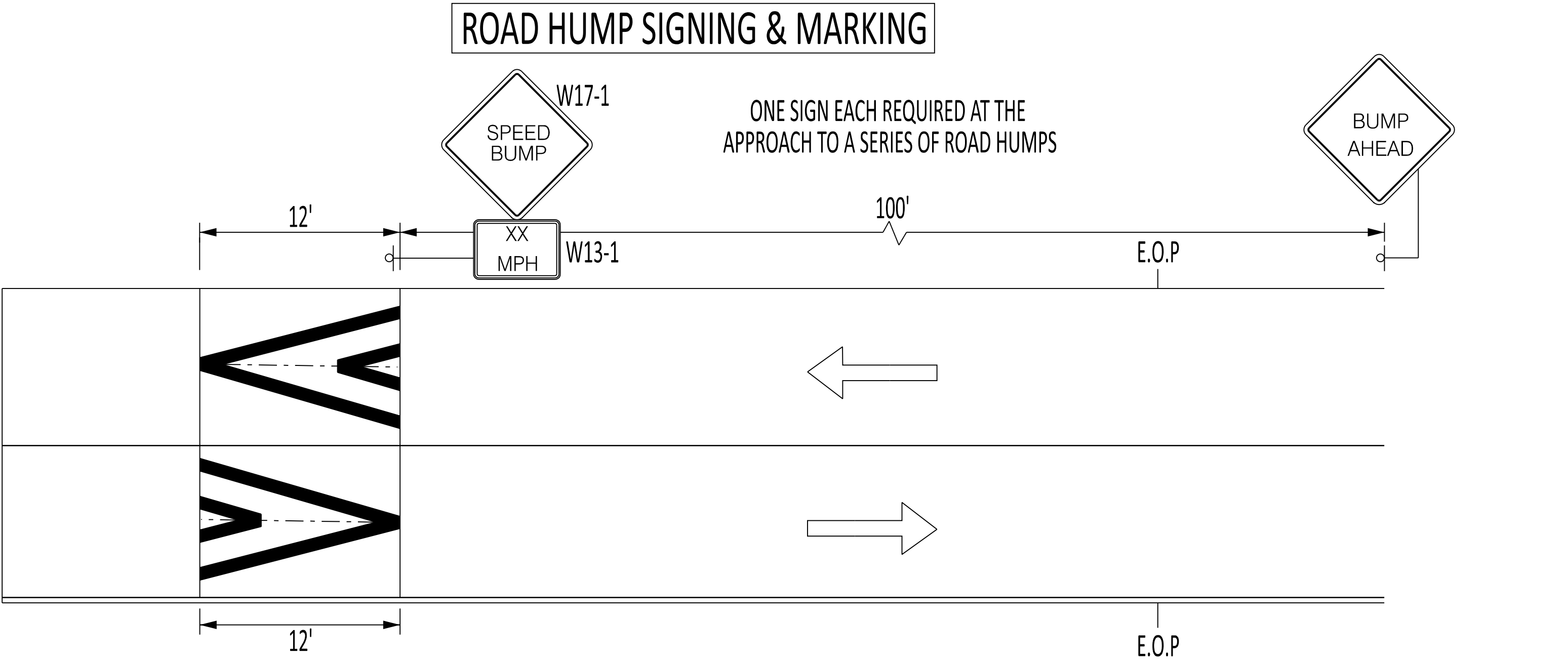
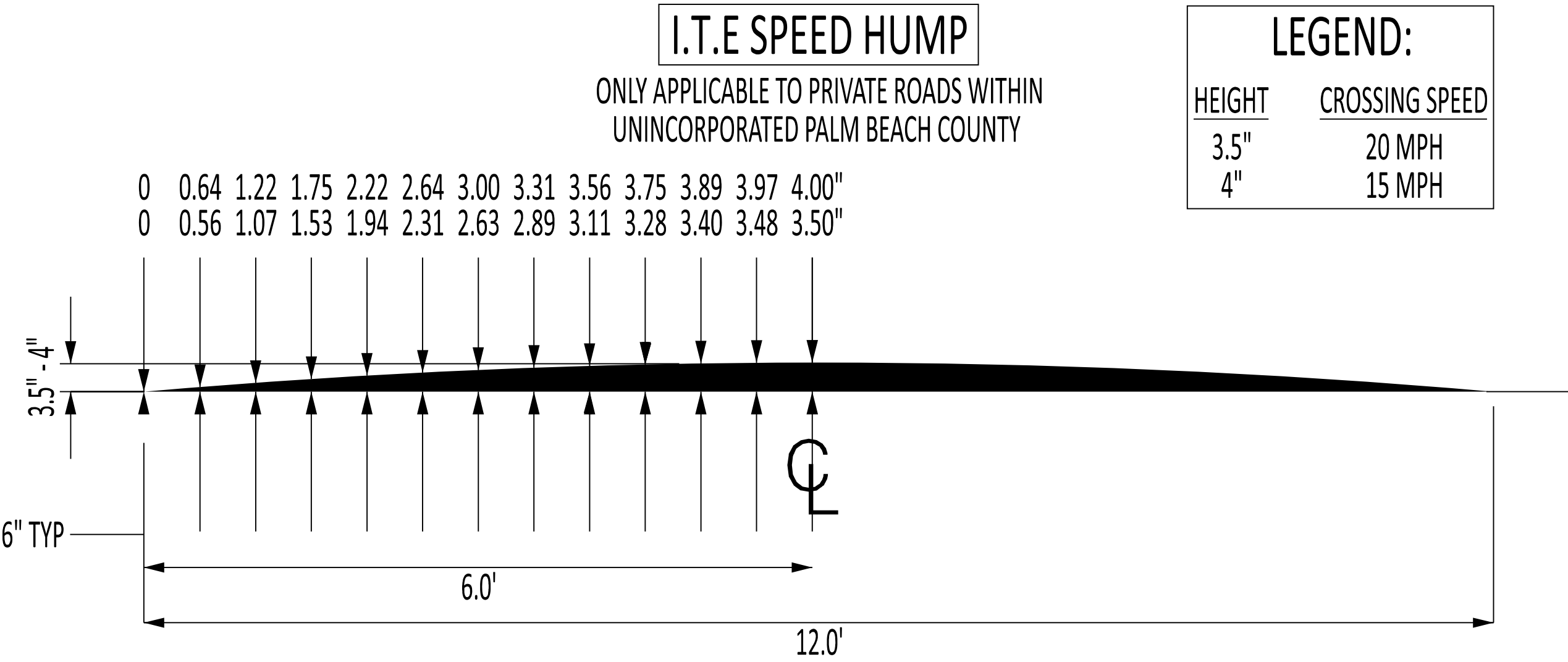
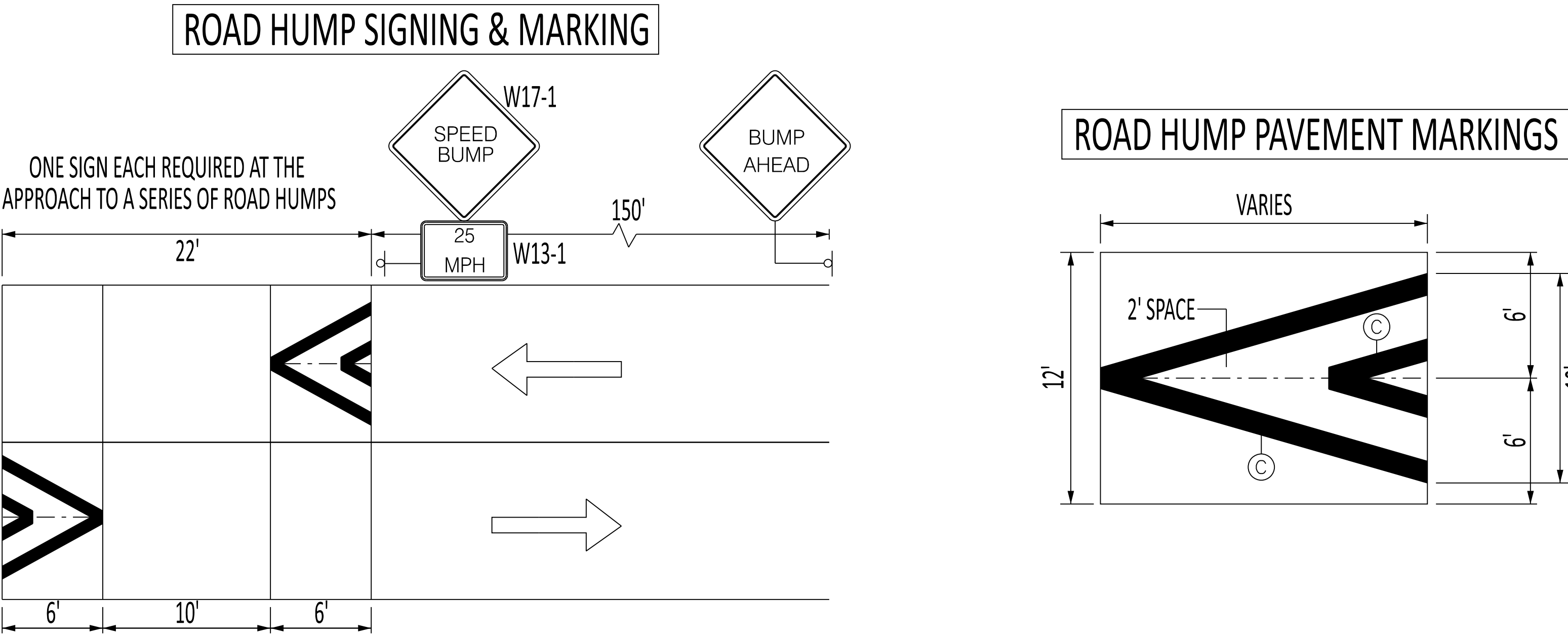
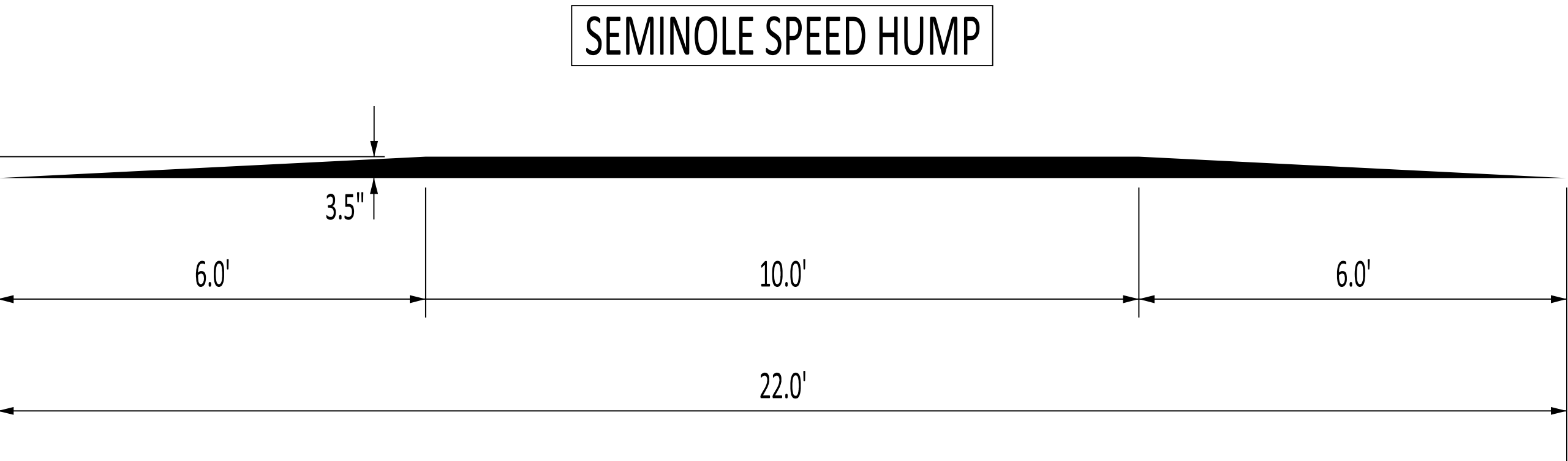
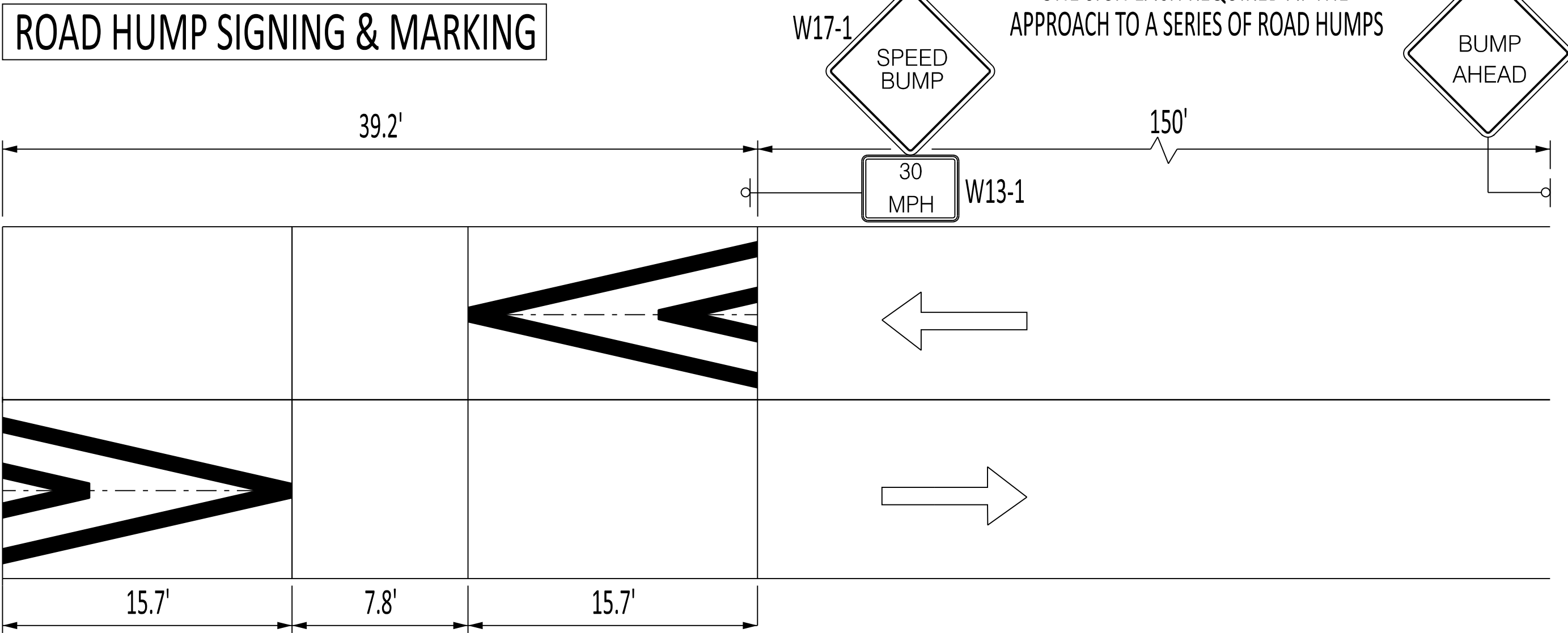
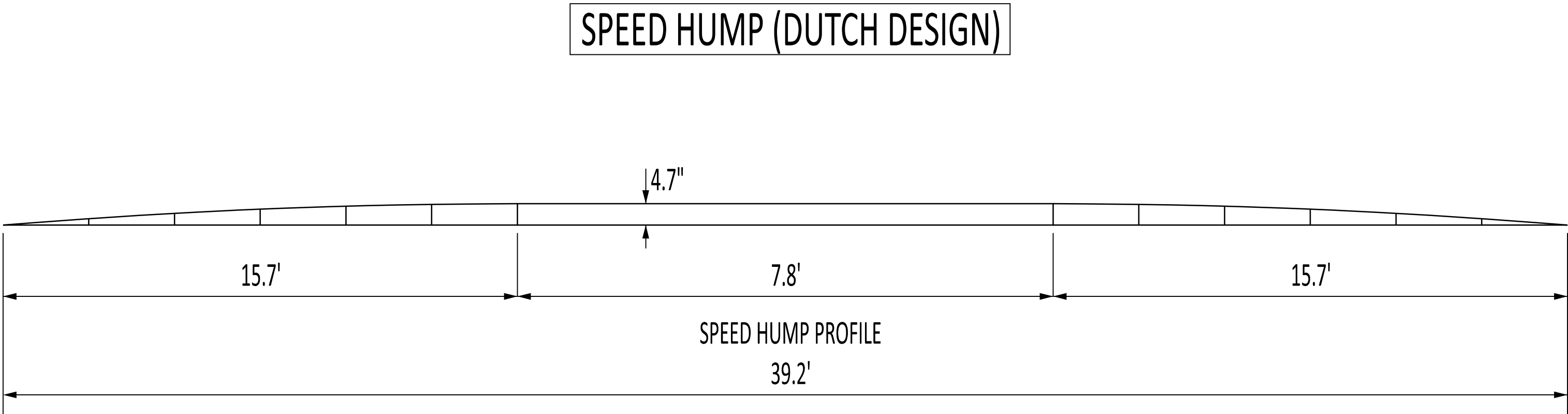


NO.	REVISION	BY	DATE

SCALE: NTS
APPROVED:
DRAWN: A.K.
CHECKED:
DATE: 7/15/24

PROJECT:
REFLECTIVE PAVEMENT MARKER
PLACEMENT DETAILS

SHEET: 6
OF: 12
T-P-24



COORDINATION WITH STREET GEOMETRY:
A THOROUGH ON-SITE ANALYSIS OF ROADWAY GEOMETRICS SHALL BE PERFORMED TO ENSURE THAT SPEED HUMPS WILL NOT BE INTRODUCED AT A CRITICAL POINT IN THE ROADWAY SYSTEM. E.G. A SEVERE COMBINATION OF HORIZONTAL, VERTICLE CURVATURE AND/OR STREET GRADIENT.

COORDINATION WITH TRAFFIC OPERATIONS:
SPEED HUMPS SHOULD NOT BE INSTALLED WITHIN 400' FT OF A TRAFFIC SIGNAL OR WITHIN 150' FROM AN INTERSECTION OR STOP SIGN. MINIMUM DISTANCE BETWEEN SPEED HUMPS IS 400' AND THE MAXIMUM SHOULD BE 800'. TO BE CONSTRUCTED ONLY WHEN APPROVED BY THE DIRECTOR OF THE TRAFFIC DIVISION.

CONSTRUCTION PROCEDURES:
IT IS RECOMMENDED THAT A TEMPLATE BE CONSTRUCTED TO VERIFY THE ACCURACY OF THE HUMP PROFILE AND TO ENSURE THAT THE DESIRED VERTICLE DIMENSIONS ARE ATTAINED WITHIN REASONABLE TOLERANCES (NORMALLY ONE-HALF INCH OR LESS, PROVIDED THE HUMP DOES NOT EXCEED 4 INCHES). IF THE PROFILE IS INCORRECT, HUMP CHARACTERISTICS WILL BE CHANGED THAT MIGHT IMPACT TRAFFIC SAFETY OR CREATE INEFFECTIVE SPEED CONTROL. IT IS RECOMMENDED THAT THE ROAD SURFACE BE EXCAVATED AT TAPERING EDGES TO PREVENT SPALLING. HUMPS MAY BE INTSALLED IN TWO LIFTS TO IMPROVE ACCURACY AND SHAPE.

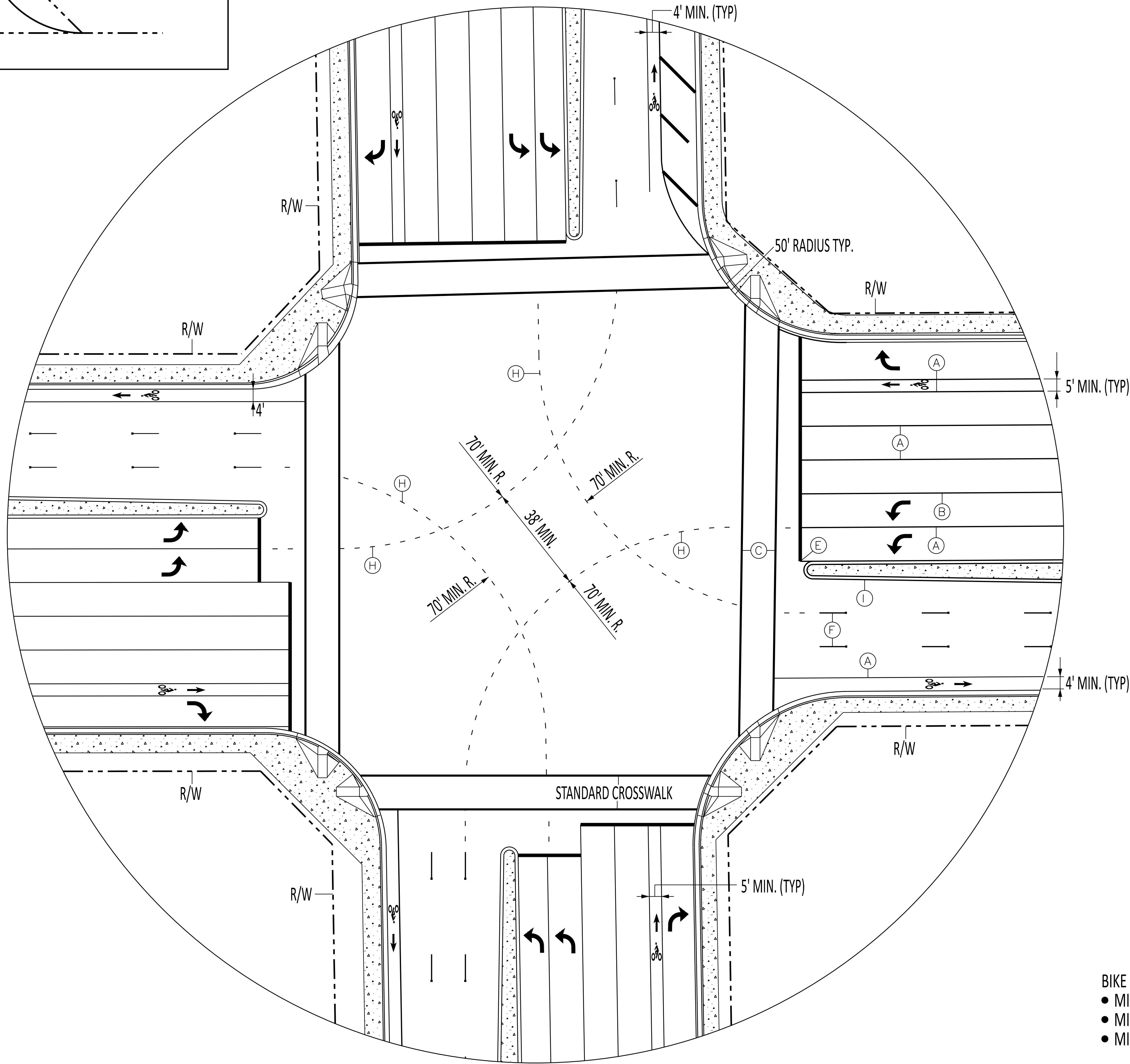
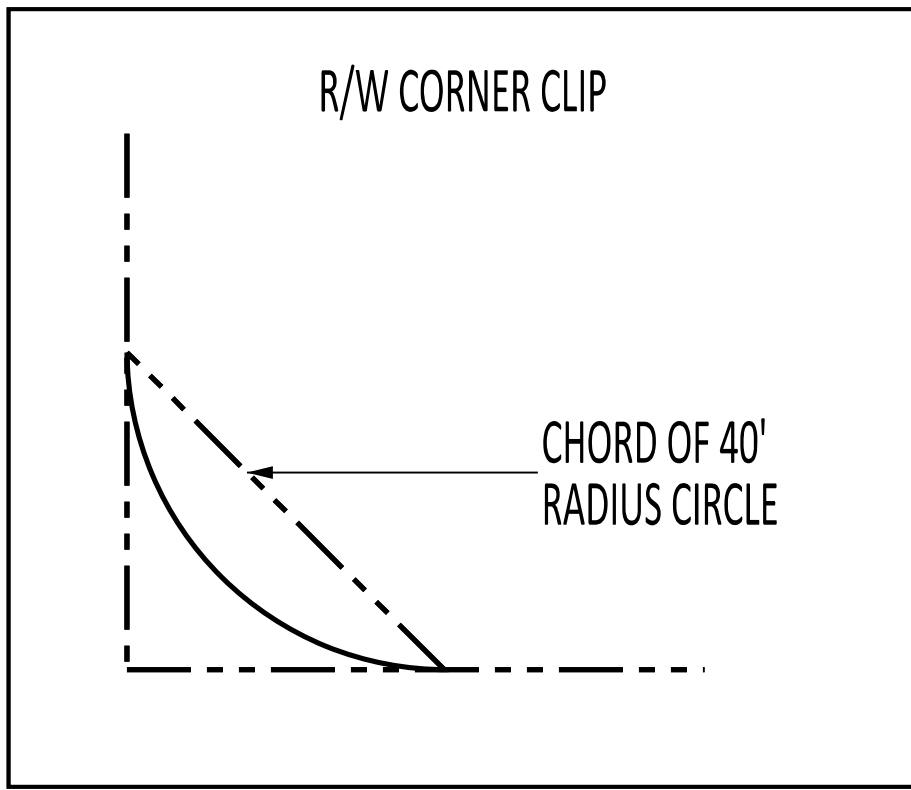
PALM BEACH COUNTY
ENGINEERING & PUBLIC WORKS
TRAFFIC DIVISION

NO.	DATE
BY	
REVISION	

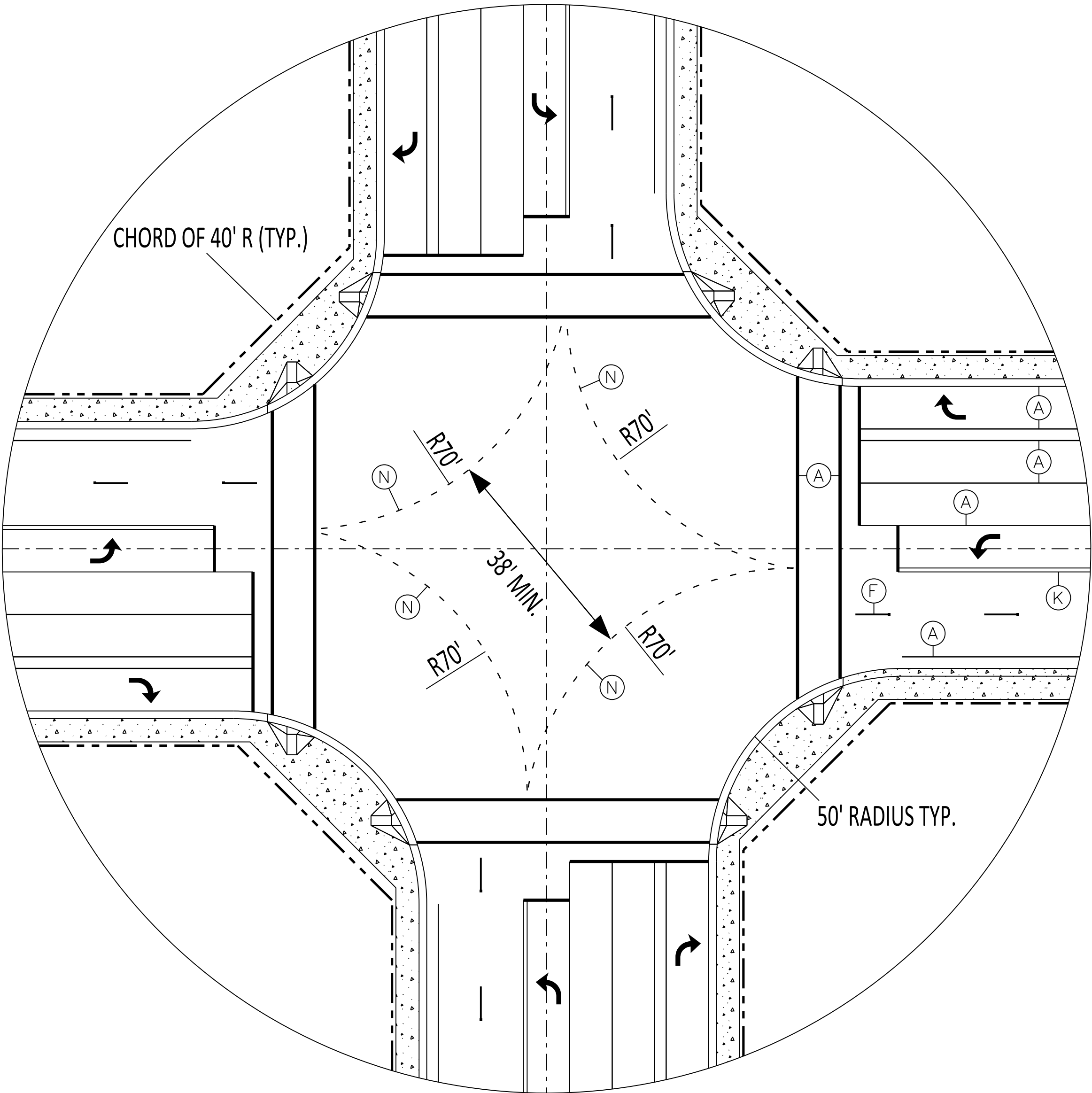
SCALE: NTS
APPROVED:
DRAWN: A.K.
CHECKED:
DATE: 7/15/24

PROJECT:
ROAD SPEED HUMP DETAILS

SHEET: 7
OF: 12
T-P-24



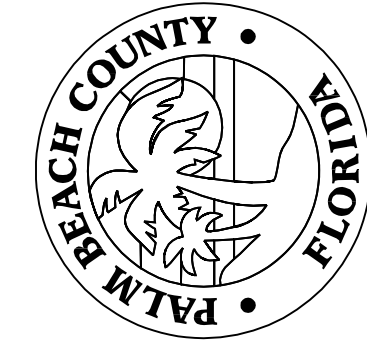
SIGNALIZED INTERSECTION
(DUAL TURN LANES)



SIGNALIZED INTERSECTION
(SINGLE TURN LANE)

NOTES:
1. FOR EXACT CROSSWALK PLACEMENT PLEASE SEE DETAIL
"EUROPEAN CROSSWALK & STOP BAR PLACEMENT ON SHEET 5

- BIKE LANES CONSIST OF THE FOLLOWING:
- MIN OF FIVE (5) FT WIDE ASPHALT.
 - MIN OF FOUR (4) FT WIDE ASPHALT WITH A TWO (2) FT CURB AND GUTTER (I.E. TYPE F OR VALLEY GUTTER).
 - MIN OF FOUR (4) FT WIDE ASPHALT WITH A ONE (1) FT SODDED, STABILIZED SHOULDER (6% OR LESS CROSS SLOPE).



NO.	REVISION	BY	DATE

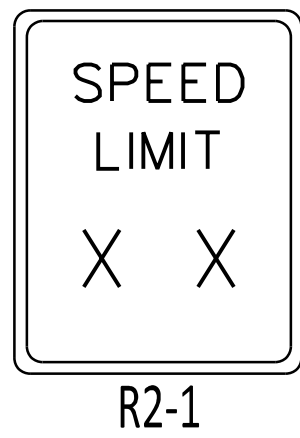
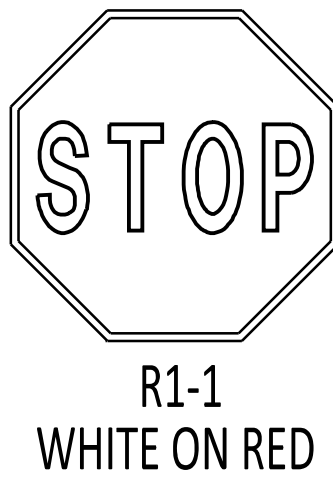
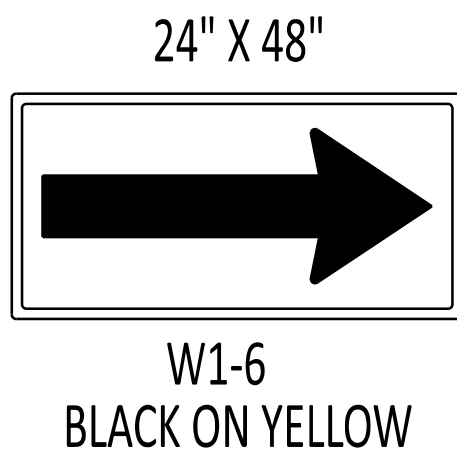
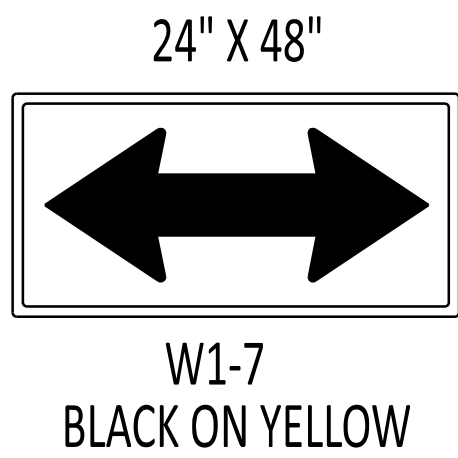
SCALE:	NTS
APPROVED:	
DRAWN:	A.K.
CHECKED:	
DATE:	7/15/2024

PROJECT:	EXPANDED SIGNALIZED INTERSECTION
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SHEET:	8
OF:	12
T-P-24	

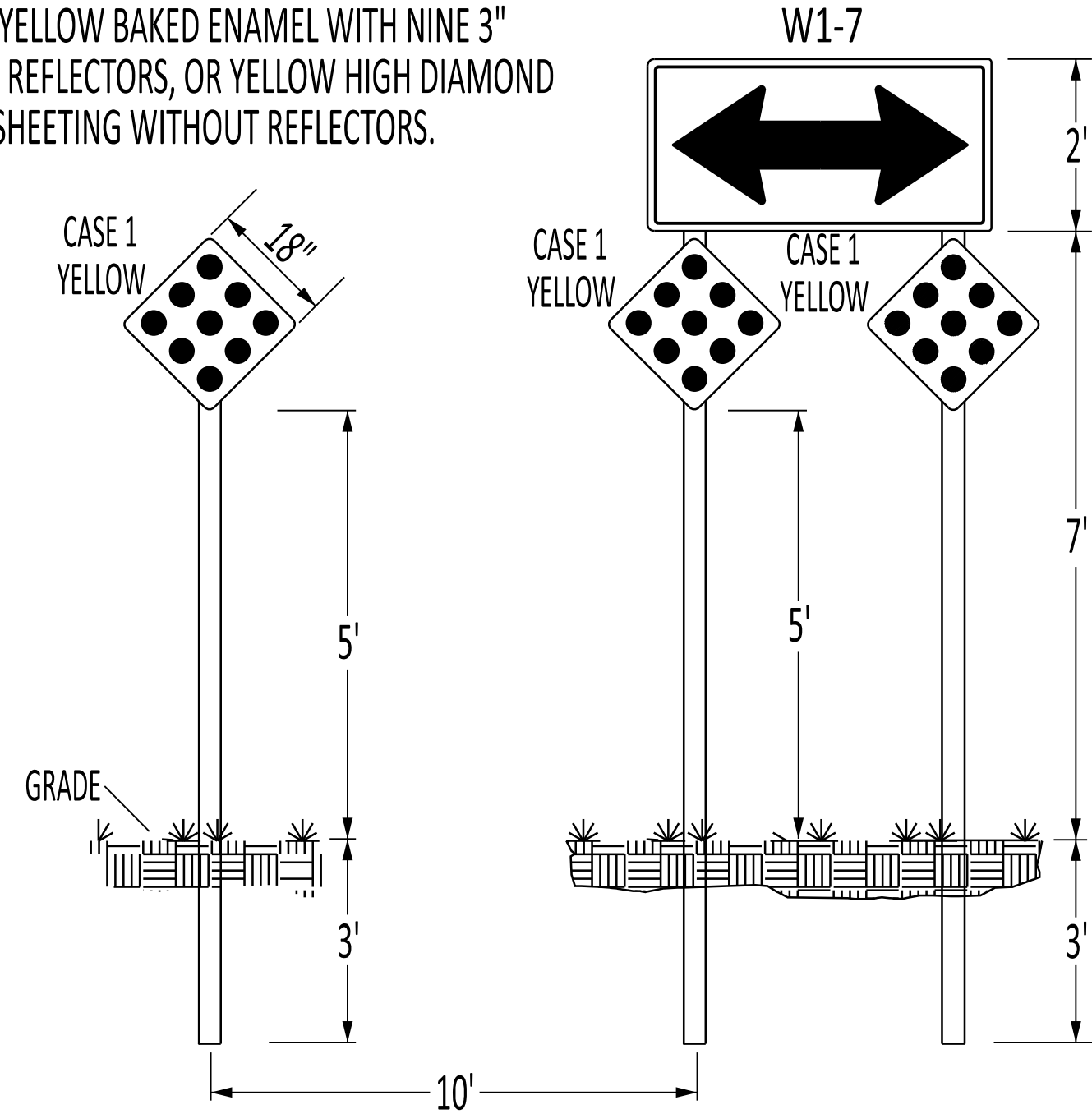
#1 SIGN SPCIFICATIONS:

SIGN FACE - DIAMOND GRADE REFLECTIVE SHEETING
SIZE - AS SHOWN
COLOR - AS SHOWN
HEIGHT - 7' MEASURED FROM BOTTOM OF THE SIGN
TO HEIGHT OF THE NEAR EDGE OF PAVEMENT, OR TO
TOP OF THE TRAFFIC SEPARATOR.
THICKNESS (ALUMINUM) : .080 MILS
THICKNESS (FIBER GLASS OR PLASTIC) : .120 MILS
BOLTS: 3/8" X 3/4" W/ HEX NUTS. 2 (MIN.) PER SIGN
POSTS: SIGNS 9 SQUARE FEET AND LARGER SHALL
BE DOUBLE POSTED.

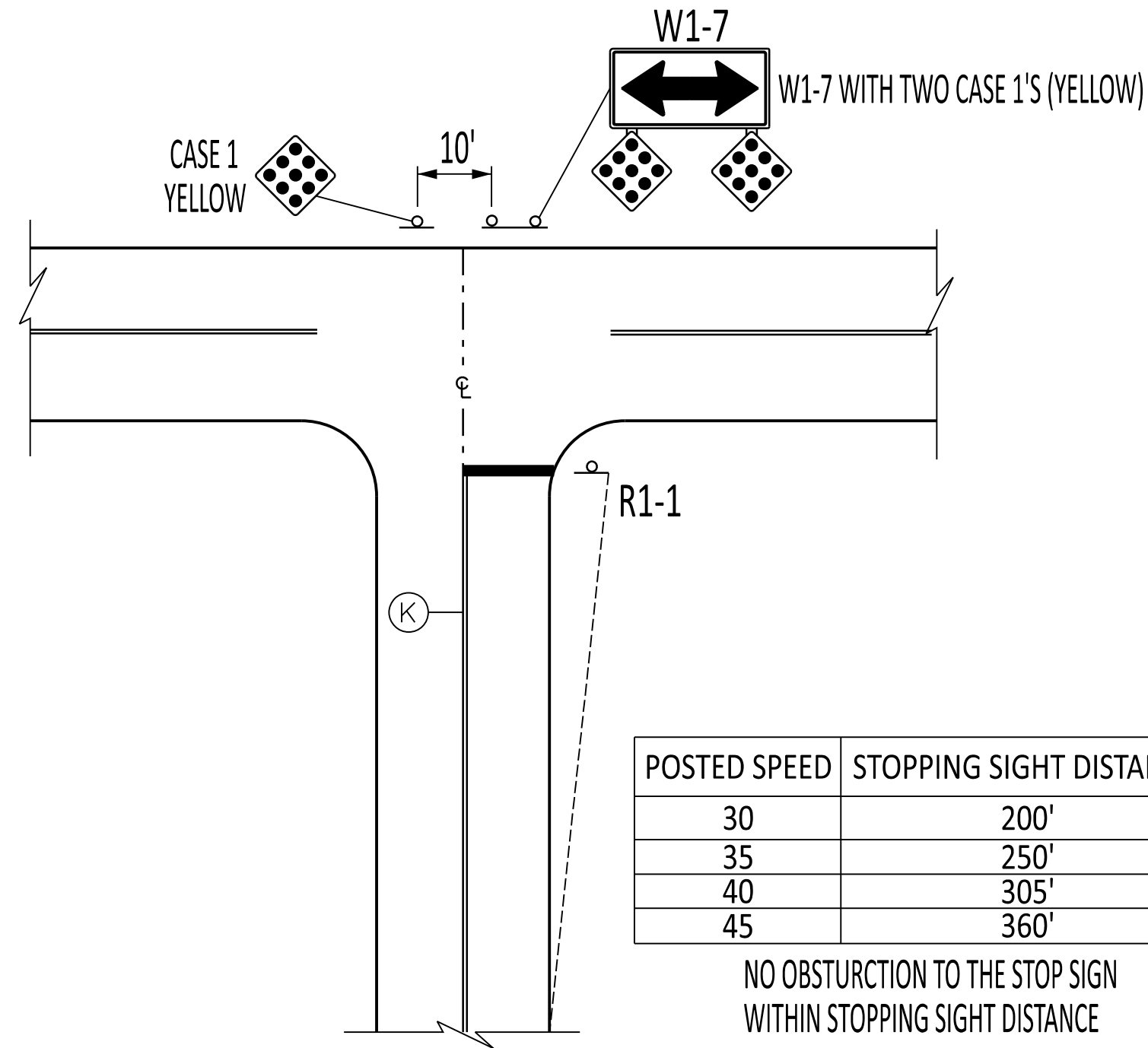


#2 T INTERSECTION

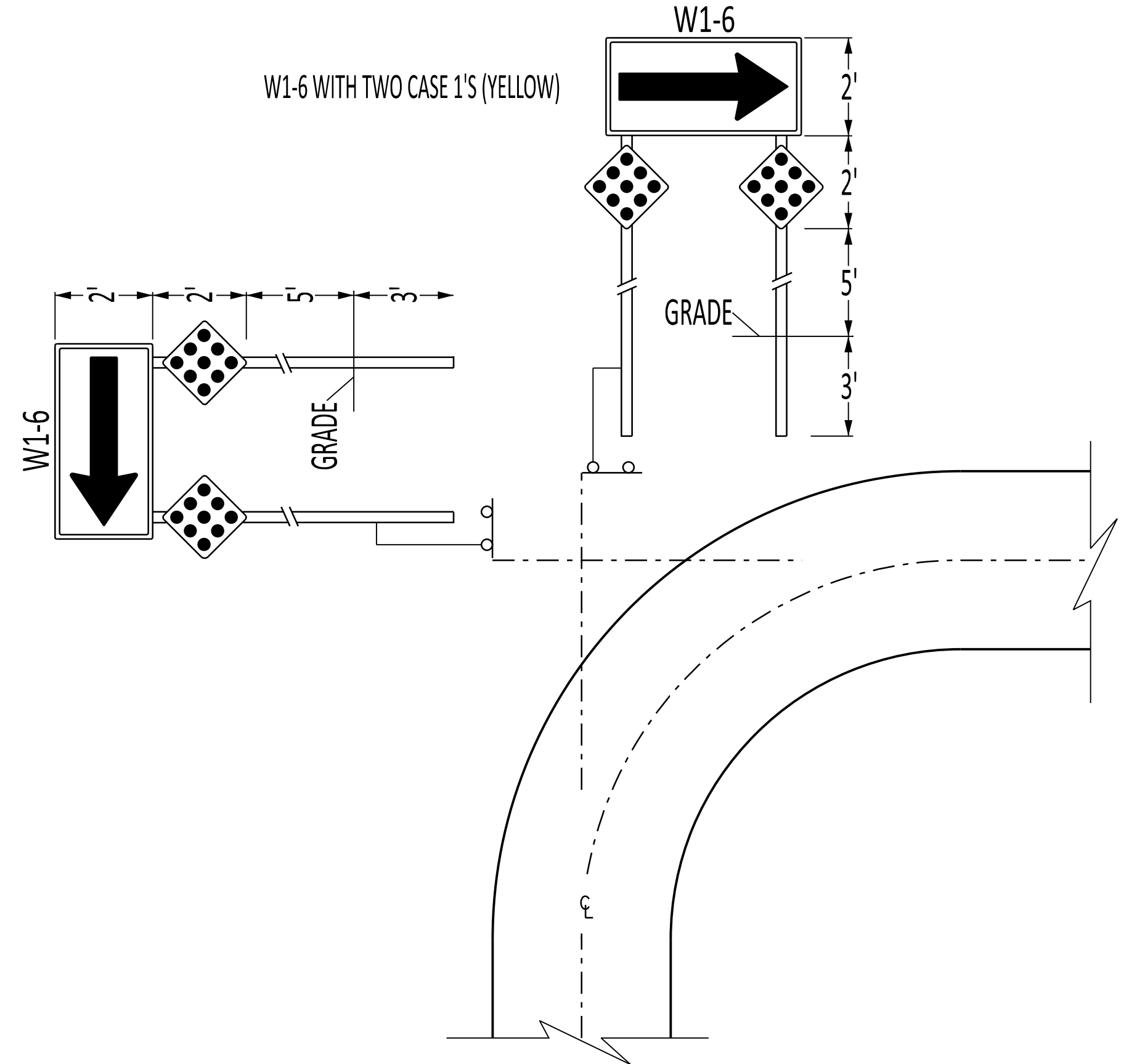
CASE 1: YELLOW BAKED ENAMEL WITH NINE 3"
YELLOW REFLECTORS, OR YELLOW HIGH DIAMOND
GRADE SHEETING WITHOUT REFLECTORS.



SIGN LOCATION AT "T" INTERSECTION
(WITH TWO PUBLIC ROADS OR PER ENGINEERING JUDGEMENT)

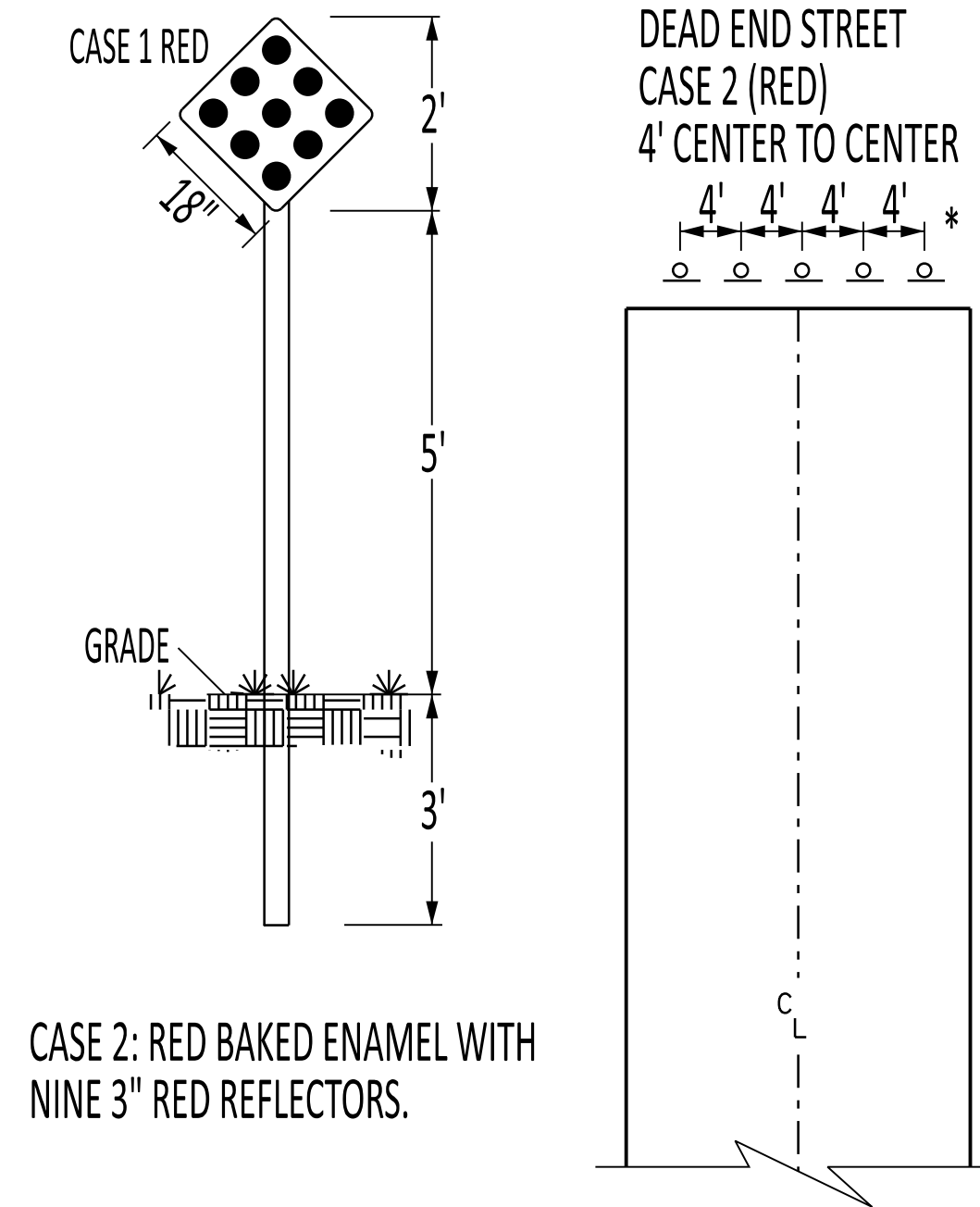


#3 SIGN LOCATION AT CURVE

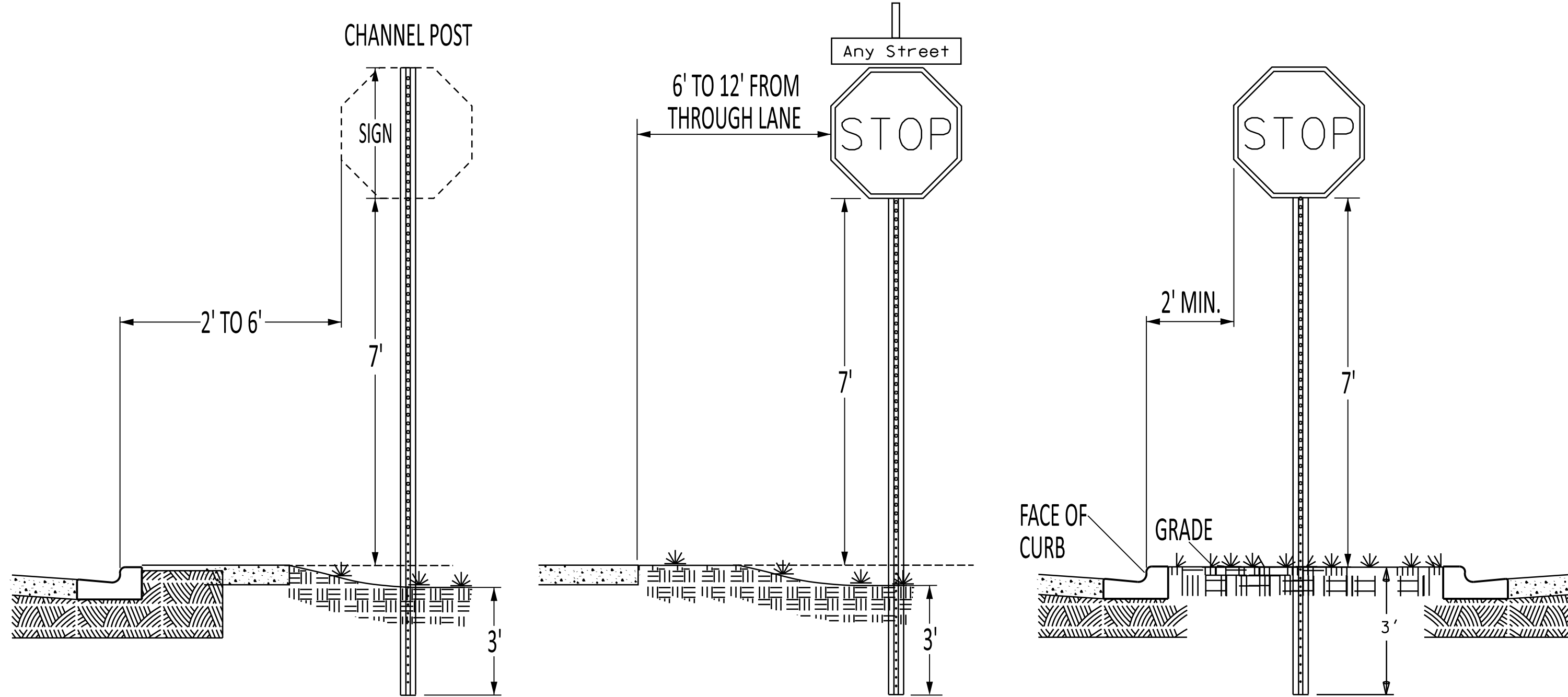


#4 DEAD ENDS

SIGN LOCATION AT
DEAD END STREET
CASE 2 (RED)
4' CENTER TO CENTER



#5 STOP SIGN PLACEMENT



#6 "NEXT SIGNAL" SIGNS:

TO BE INSTALLED ON THOROUGHFARE ROADWAYS WITH 4 OR MORE LANES, IN MEDIAN (IF
POSSIBLE) OR ON THE SIDE NOT TO OBSTRUCT PATHWAY. APPROXIMATELY 600 FEET IN
ADVANCE OF INTERSECTION CROSSWALKS.

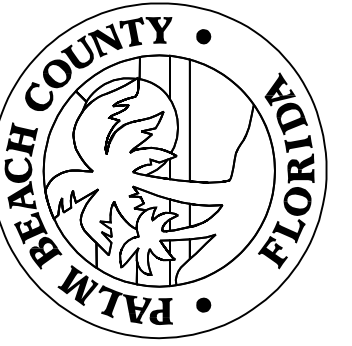
COLOR: WHITE LETTERS ON GREEN BACKGROUND. WHITE 1" BORDER.

BLANK SIZE: 30" X 60" FOR MEDIANS.

LETTER SIZE: 10" UPPERCASE AND 8" LOWERCASE FOR STREET NAME "NEXT SIGNAL" PHRASE
5" UPPERCASE.

* - WITHIN RESIDENTIAL COMMUNITIES
ONLY 3 SIGNS ARE REQUIRED

POST: STEEL FLANGED CHANNEL POST W/ BAKED GREEN ALKYD OR GALVINIZED FINISH PER A.S.T.M. - A - 123 ;
MIN. WEIGHT: 3 LBS. PER FOOT, 7/16" DIA. HOLES, 1" APART TOP TO BOTTOM.

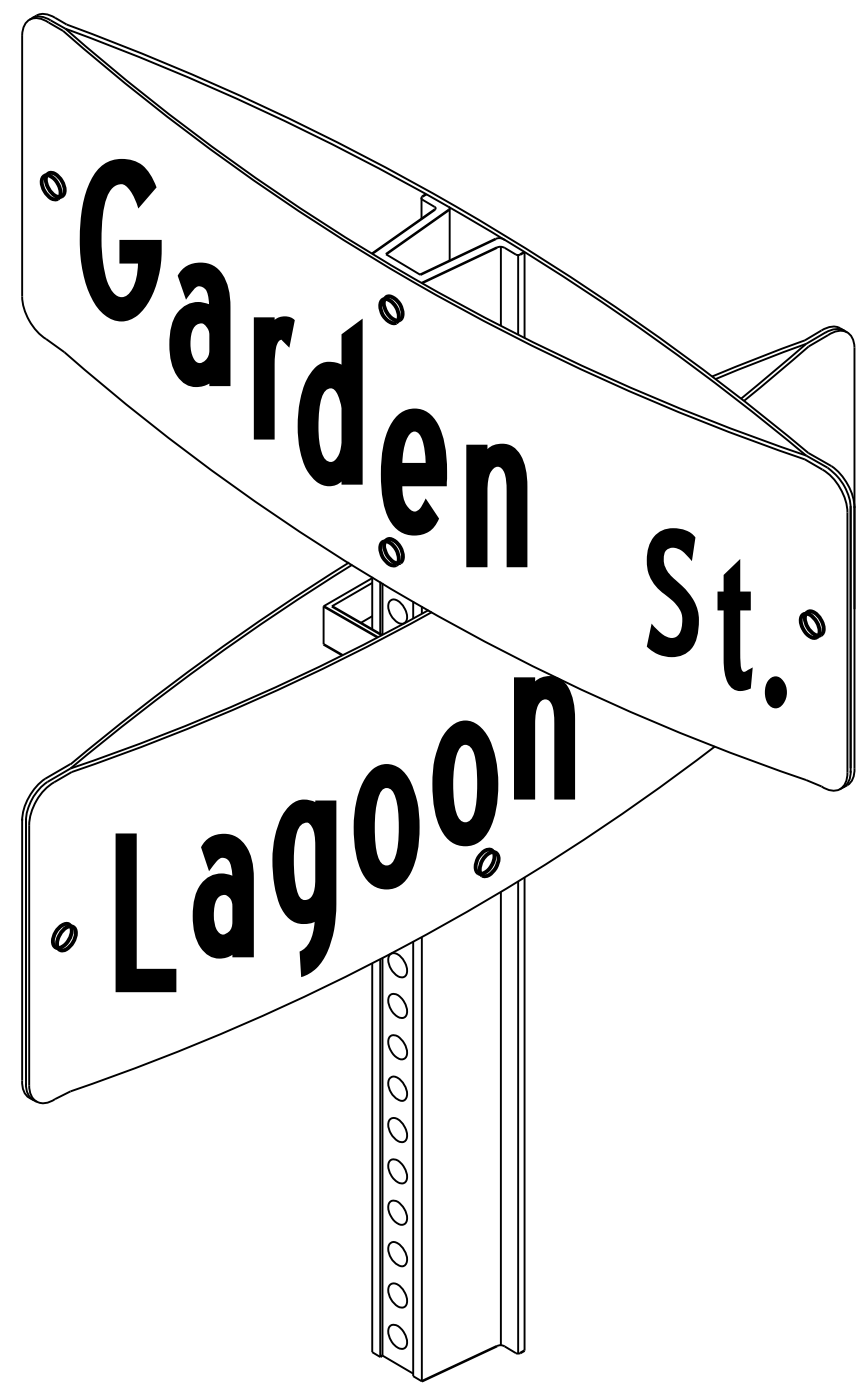


NO.	REVISION	BY	DATE

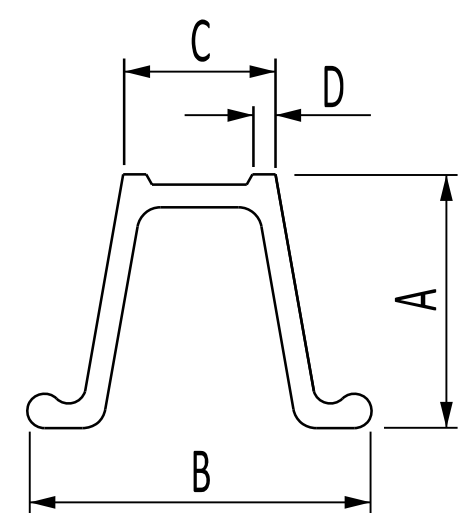
SCALE:	NTS
APPROVED:	
DRAWN:	A.K.
CHECKED:	
DATE:	7/15/24

PROJECT:	SIGN PLACEMENT
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SHEET:	10
OF:	12
T-P-24	

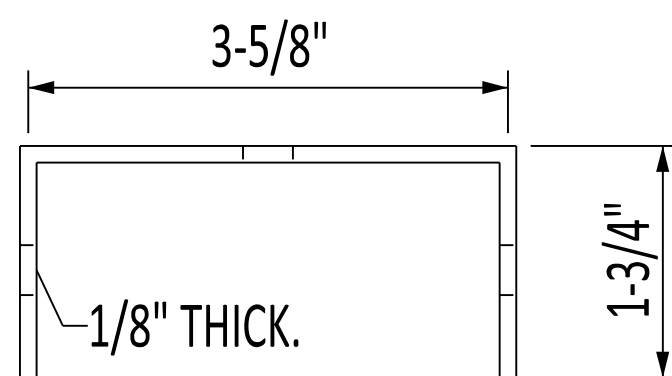
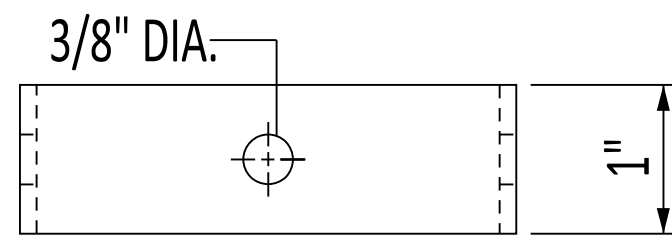
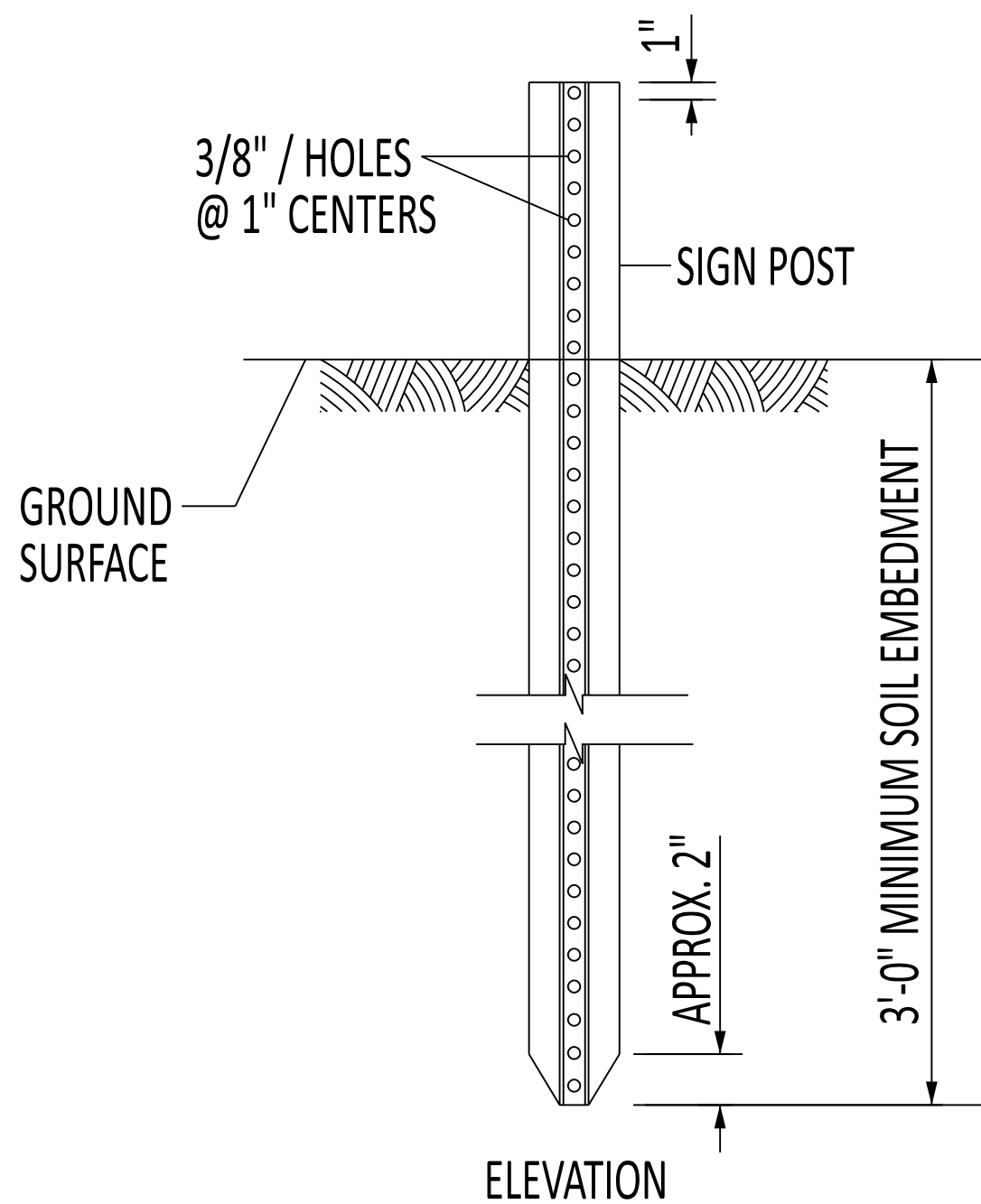


ISOMETRIC VIEW

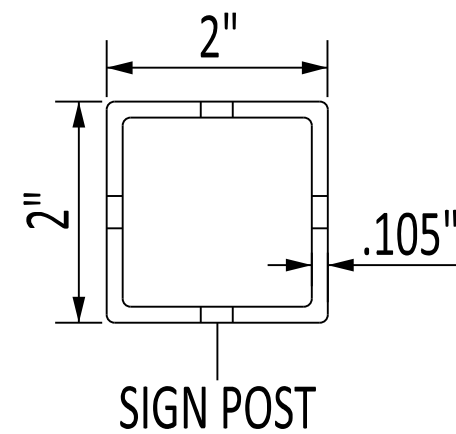
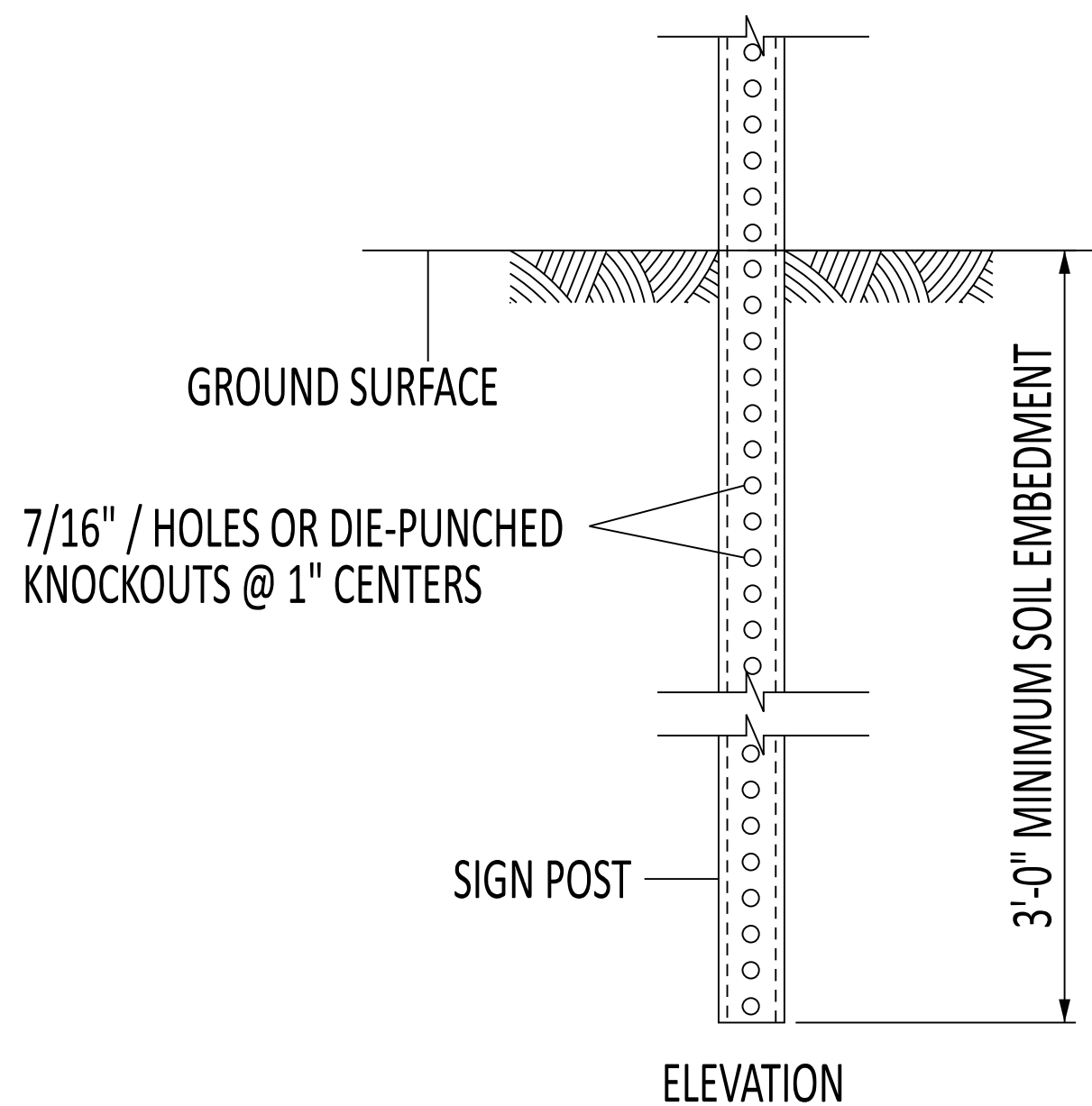


'A'	1.875"
'B'	3.5"
'C'	1.313"
'D'	0.22"

CHANNEL POST



STEEL FLANGED CHANNEL POST DETAILS



STEEL SQUARE TUBE POST DETAILS

GENERAL SPECIFICATIONS:

FLAT BLADE: ALCOA *86054.6063-T6 ALLOY, ETCHED, DEGREASED WITH *1200 ALODINE FINISH WITH *3877 GREEN DIAMOND GRADE BACKGROUND AND EQUAL DIMENSIONS - 9" & 12" MIN. H, 24", 30", 36" AND 42" L.

LETTERS: NAME - 6" UPPERCASE WITH 4.5" LOWERCASE & 9" UPPERCASE WITH 6.75" LOWERCASE, SERIES 'B' * 3870 DIAMOND GRADE (SILVER) OR EQUAL - SUFFIX - 4.5".

POST: STEEL FLANGED CHANNEL POST 3 LBS. WEIGHT PER FOOT WITH BAKED GREEN ALKYD OR GALVANIZED FINISH PER A.S.T.M. - A - 123 WITHOUT ANCHOR PLATES. SQUARE POST PER FDOT INDEX 700-010.

BRACKETS SHALL BE ATTACHED FIRMLY ON STANDARD SQUARE TUBE OR U-CHANNEL POSTS BY MEANS OF (2) 5/16" END VIEW DIAMETER HEX HEAD BOLTS.

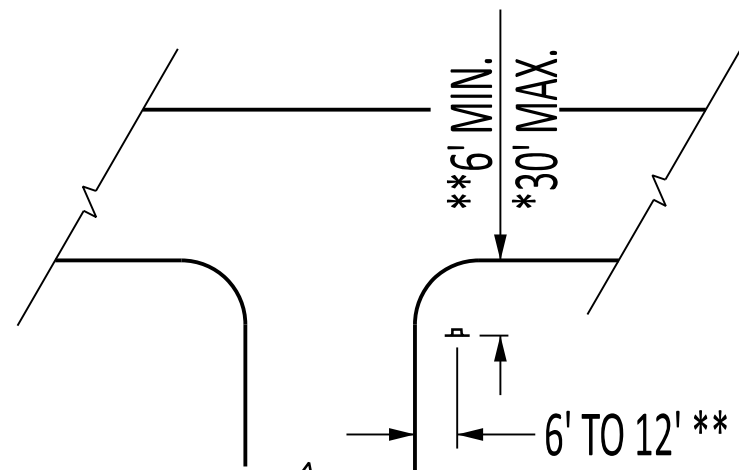
STOP SIGN: R1-1 MUTCD DIAMOND GRADE.

LOCATION: ONE PER INTERSECTION AS INDICATED ON THE PLANS.

POLICY: 9" BLADES WITH 6" UPPERCASE WITH 4.5" LOWERCASE LETTERS FOR TWO LANE ROADS WITH A SPEED LIMIT UNDER 45 MPH.

12" BLADES WITH 9" UPPERCASE WITH 6.75" LOWERCASE LETTERS FOR:

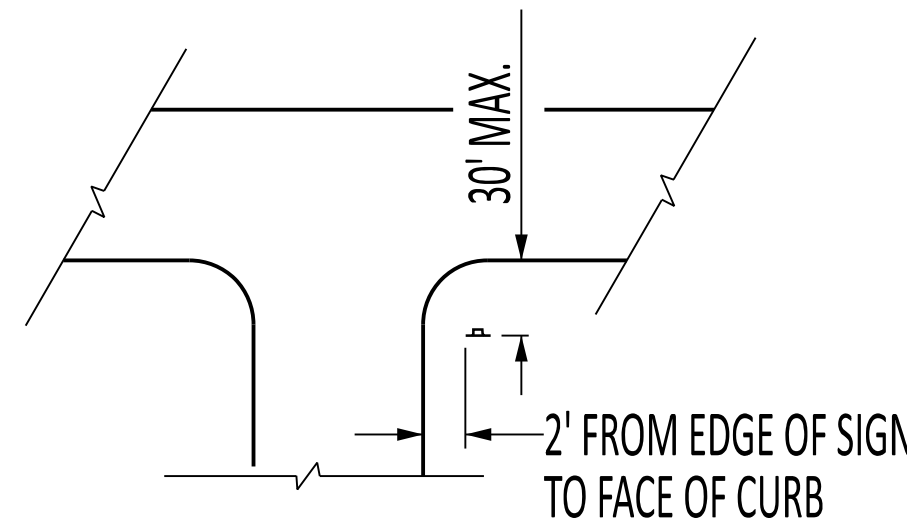
- THOROUGHFARE ROADS FOUR LANES OR WIDER.
- TWO LANE ROADS WITH A POSTED SPEED LIMIT OF 45 MPH OR MORE. ONLY ONE BLADE WILL BE INSTALLED AT INTERSECTION WITH THOROUGHFARE ROAD INDICATING THE SIDE STREET NAME.



* AT RADIUS WHEN POSSIBLE
30' MAX FROM THROUGH LANE

UNCURBED SECTION

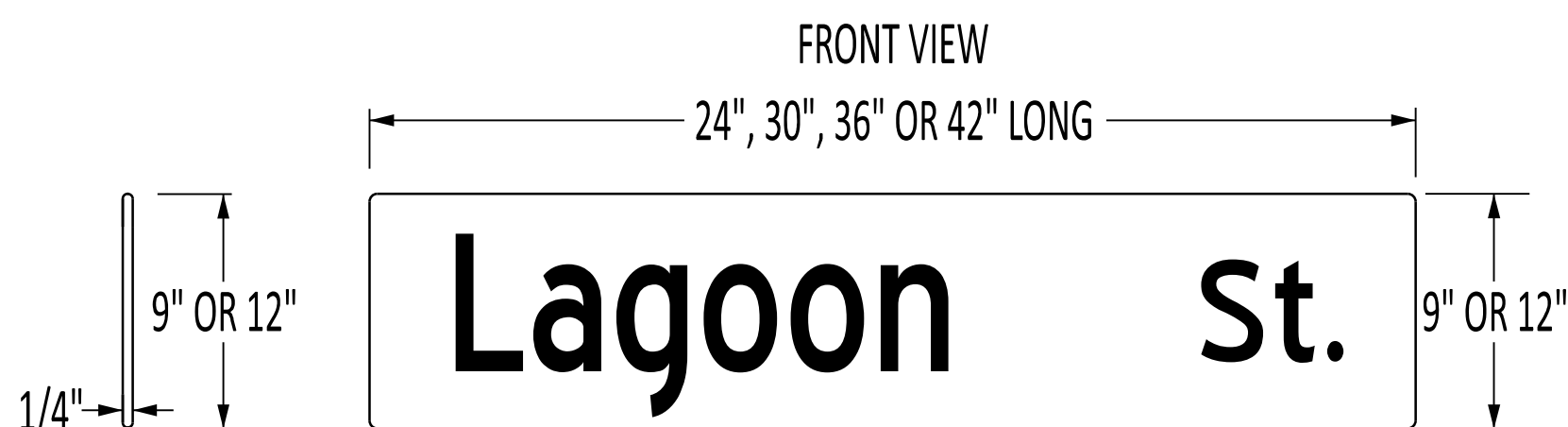
** FROM THROUGH LANE



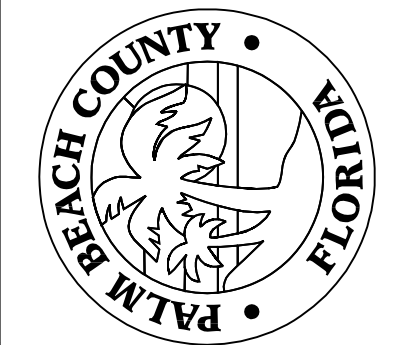
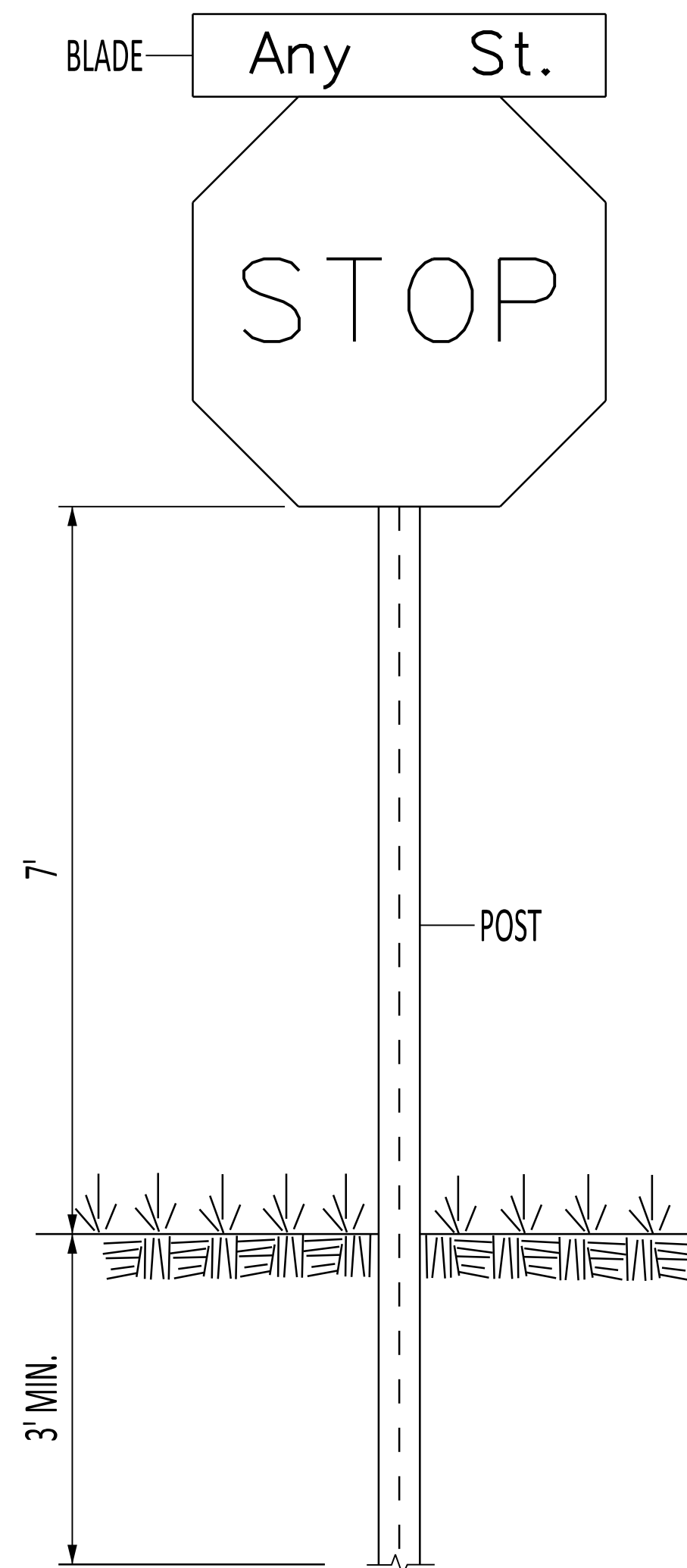
CURBED SECTION

IF STOP BAR IS USED IT SHALL
BE PLACED AT THE STOP SIGN

TYPICAL STOP SIGN PLACEMENT



STREET NAME SIGN
D3-1



NO.	DATE
BY	
REVISION	

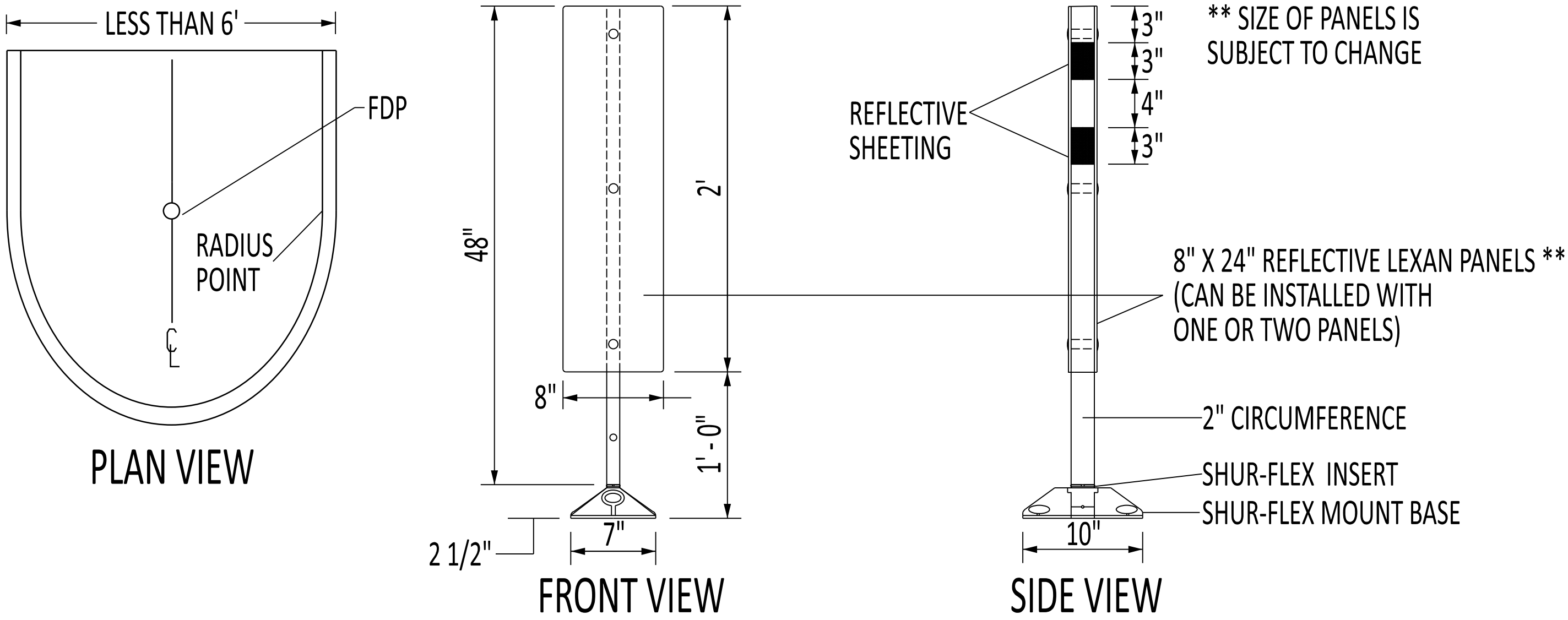
SCALE:	NTS
APPROVED:	
DRAWN:	A.K.
CHECKED:	
DATE:	7/15/24

PROJECT: STREET NAME SIGN
WITH STOP SIGN

SHEET:	11
OF:	12
T-P-24	

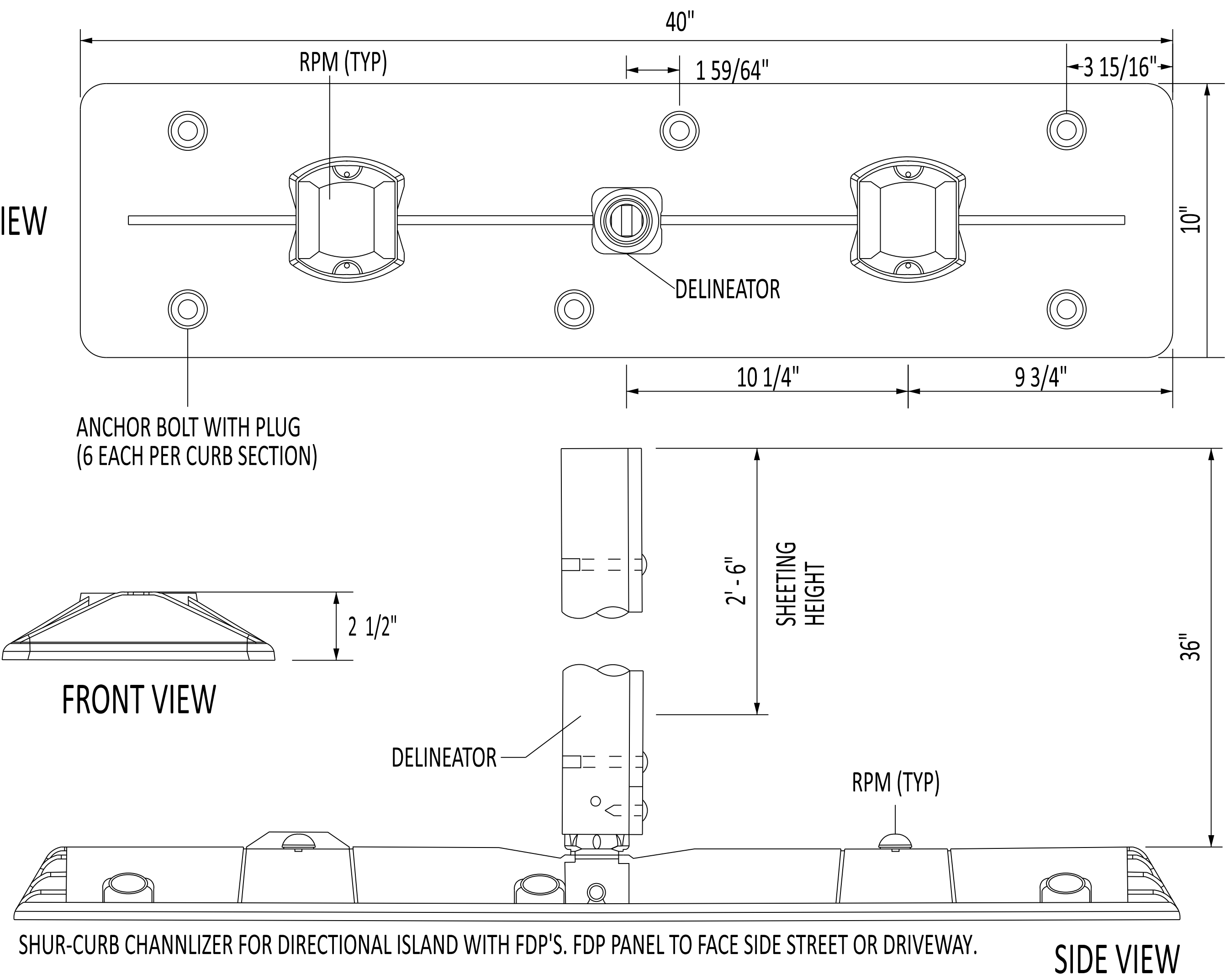
#1

FLEXIBLE DELINEATOR POST (FDP)
(SHUR-FLEX HIGH VISIBILITY DELINEATOR)

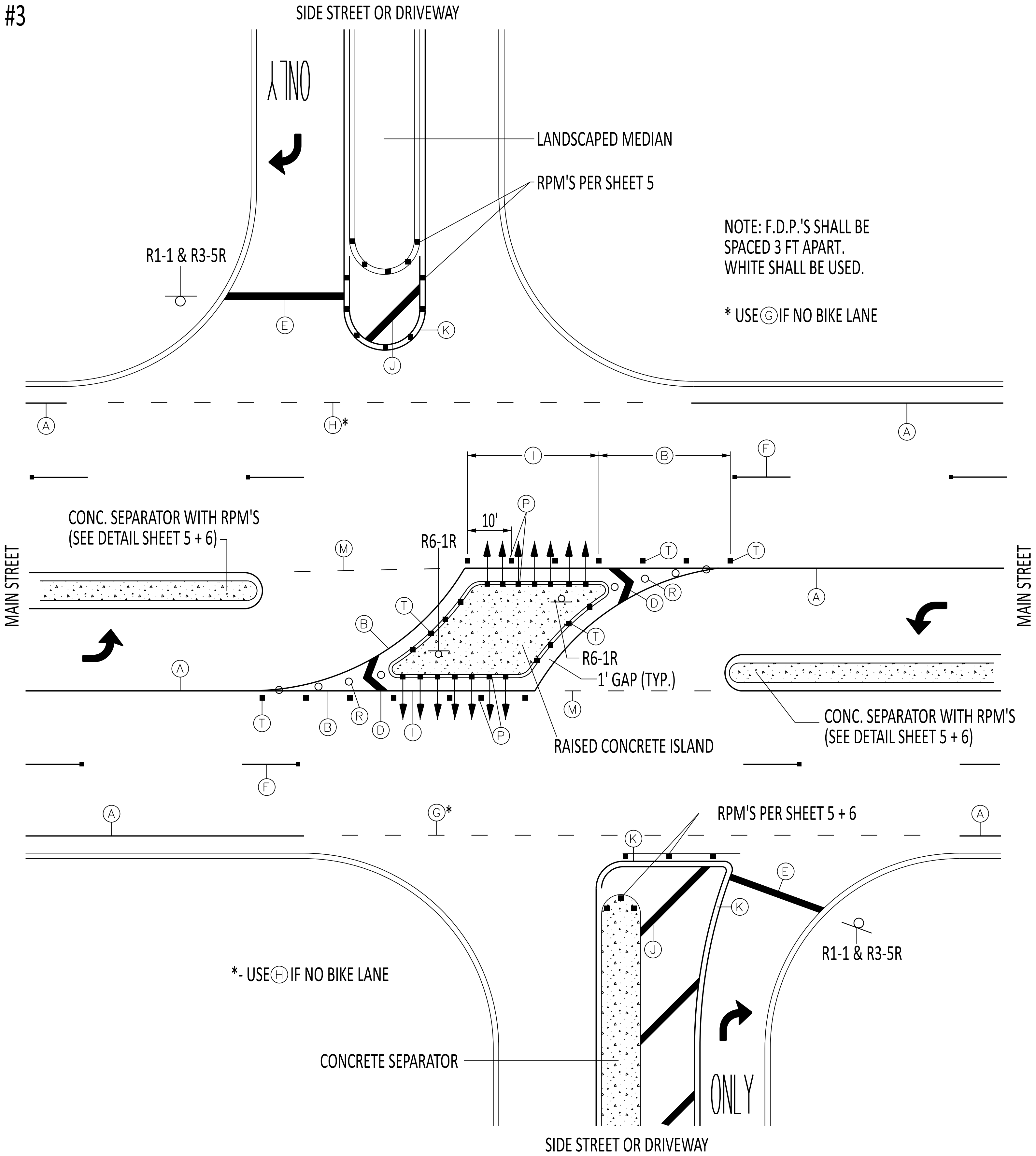


#2

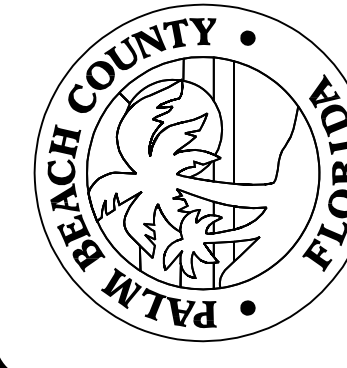
PLAN VIEW



#3



1. THE FDP SHALL MATCH THE ADJCENT EDGLINE COLOR (YELLOW OR WHITE ONLY).
2. WHEN SHOWN ON PLANS, FDP'S SHALL BE IDENTIFIED IN A SIMILAR MANNER TO THAT SHOWN ON THIS TYPICAL. COLORS AND NUMBER OF EACH MUST BE SHOWN ON ALL SUMMARY OF QUANTITIES SHEETS. ALSO, COLOR MUST BE SPECIFIED FOR EACH FDP GROUP ON THE PLANS.
3. ON DIRECTIONAL ISLANDS THE FDP'S ARE REQUIRED TO SUPPLEMENT THE EXISTING SIGNING, MARKINGS, AND RPM'S TO DETER WRONG WAY MANEUVERS. FDP'S SHOULD BE INSTALLED MIDWAY BETWEEN THE STRIPED CHEVRONS.
4. SHUR-FLEX HIGH VISIBILITY FLEXIBLE DELINEATOR (FDP) SHALL BE CONSTRUCTED FOR MEDIANS LESS THAN 6' WIDE. FOR MEDIANS AT LEAST 6' WIDE INSTALL R4-7C SIGN WITH CASE 1 YELLOW REFLECTORS ONLY.
5. SHEETING TYPE: DIAMOND GRADE SERIES 3900 SERIES TYPE 3.



NO.	REVISION	BY	DATE

SCALE: NTS
APPROVED: A.K.
DRAWN: A.K.
CHECKED: 7/15/24
DATE: 7/15/24

FLEXIBLE DELINEATOR POST
PLACEMENT DETAIL
DIRECTIONAL ISLAND

PROJECT:

SHEET: 12
OF: 12
T-P-24